

Committee	STRATEGIC PLANNING COMMITTEE	
Report Title	PLOT 08, CONVOYS WHARF, LONDON, SE8 3JH	
Ward	Evelyn	
Contributors	David Robinson	
Class	PART 1	09 06 2020

Reg. Nos. DC/18/107698

Application dated 20 June 2018

Applicant DP9 Limited, on behalf of Convoys Wharf Limited

Proposal Approval of Reserved Matters (layout, scale, appearance, access and landscaping) for Plot 08 (Phase 1) comprising the construction of a development plot ranging between 2, 10, 12 and 14 storeys in height, providing 456 residential units, 1,650 sq. m (GEA) of retail and café/restaurant uses (Class A1 to A4), car parking, cycle storage, servicing, plant area, new landscaping and other details pursuant to condition 20(i) and 21(i) (Reserved Matters/Approval of Details) together with discharge/approvals under condition 3(ii) (Microclimate: Wind), condition 7 (Building Design Statement), condition 8(i) (Reconciliation Document), condition 10 (Housing 'Residential Space Standards'), condition 12(i) (Lighting), condition 13 (Heritage Statement), condition 14(i) (Biodiversity), condition 15 (Energy Strategy), condition 19 (Drainage and Flood Risk), condition 33 (Details of Cycle Parking), condition 45(i) (Contaminated Land), condition 47 (Surface Water Control Measures), and condition 50(i) (Electric Vehicle Charging Points) of Outline Planning Permission ref. DC/13/83358 for the comprehensive redevelopment of Convoys Wharf, Prince Street, London, SE8 3JH.

Applicant's Plan Nos. CON4-PA-03-011; CON4-PA-09-304; CON4-PA-09-303; CON4-PA-09-302; CON4-PA-09-301; CON4-PA-09-300; CON4-PA-07-101; CON4-PA-07-100; CON4-PA-03-013; CON4-PA-03-012; CON4-PA-05-303-A; CON4-PA-05-302-A; CON4-PA-05-301-A; CON4-PA-05-300-A; CON4-PA-05-204-A; CON4-PA-05-203-A; CON4-PA-05-202-A; CON4-PA-05-201-A; CON4-PA-05-109-A; CON4-PA-05-108-A; CON4-PA-05-107-A; CON4-PA-05-106-A; CON4-PA-05-105-A; CON4-PA-05-104-A; CON4-PA-05-103-A; CON4-PA-05-102-A; CON4-PA-05-101-F; CON4-PA-05-100-F

P10852-00-002-320 D00; P10852-00-002-130 D01; P10852-00-002-121 D01; P10852-00-002-120 D01; P10852-00-002-112 D01; P10852-00-002-111 D01; P10852-00-002-110 D01; P10852-00-002-109 D01; P10852-00-002-108 D01; P10852-00-002-105 D01; P10852-00-002-104 D01; P10852-00-002-103 D01; P10852-00-002-102 D01; P10852-00-002-101 D01;

P10852-00-002-100 D01; CW-P08-GL-DS-0510-300-PL; P10466-00-002-800-D01; P10466-00-002-801-D01; P10852-00-002-100-MARK-UP-FOOTWAY-WIDTHS

Code of Construction Practice Rev 3 (February 2018) (AECOM); Building Design Statement Rev 1 (February 2018) (Farrells); Lifetime Homes Compliance Rev 1 (February 2018) (Farrells); Remediation Strategy (February 2018) (AECOM); Noise Assessment (February 2018) (Cundall); Transport Statement (February 2018) (AECOM); Foundation Method Statement Rev 1 (February 2018) (AECOM); Structural Survey (February 2018) (AECOM); Ecology Statement (February 2018) (AECOM); Statement of Community Involvement (February 2018) (Quatro); Archaeology Evaluation Statement (February 2018); EIA Statement of Conformity (January 2018) (AECOM); Healthy Streets Assessment (November 2019) (AECOM); Response to LBL contaminated land comments (June 2019) (AECOM); Phase 1 Desk Study Summary for Plot 08 (September 2019) (AECOM); Phase 2 Intrusive Site Investigation Summary for Plot 08 (October 2019) (AECOM); Plot 08 Transport Statement Addendum (June 2019) (AECOM); Heritage Statement (October 2019) (CGMS); Biodiversity Report (February 2018) (AECOM); Energy & Sustainability Statement (February 2018) (AECOM); Design & Access Statement Rev 1 (February 2018) (Farrells); Planning Statement (June 2018) (DP9); Reconciliation Statement (February 2018) (DP9); Wind Assessment (February 2018) (AECOM); Daylight & Sunlight Assessment (February 2018) (Farrells); Ventilation Statement Rev B (February 2018) (Cundall); Lighting Strategy (February 2018) (AECOM); Supplement Planning Report Rev A (August 2018) (Farrells); Substituted plans (Rev A) (July 2018) (Farrells); Temporary/Permanent Works (November 2019) (Gillespies); Response to LBL wind comments (October 2019); DAS Addendum (August 2019) (Farrells); Planning Statement Rev01 (August 2018) (DP9); Initial Cultural Strategy Response Statement (November 2019) (Farrells); Heritage Design Proposal Rev 1 (December 2019) (Really Local); Summary of Design Response to Heritage Assets Rev B (January 2020) (Farrells); Heritage Statement (October 2019) (CGMS); Building External Materials (Farrells)

Background Papers

Case File LE/451/179/TP
NPPF (February 2019)
London Plan (March 2016)
Core Strategy (June 2011)
Development Management Local Plan (November 2014)

Designation

PTAL 1a / 2 / 3
Area of Archaeological Priority
Deptford Neighbourhood Forum
Flood Risk Zones 2 and 3
Landmark Viewing Corridor
Protected Vista
Regeneration and Growth Area
Site of Importance for Nature Conservation
Strategic Site Allocation 2
Thames Policy Area

1.0 SUMMARY

- 1.1 This report relates to an application for the approval of Reserved Matters and other details relating to Plot 08 within the Convoys Wharf Development.
- 1.2 The application has been brought before members for a decision due to the number of objections (81 no.).

2.0 INTRODUCTION

Background to Outline Permission at Convoys Wharf

- 2.1 The relevant planning history is set out in Section 4 of this Report. By way of further background, the outline planning permission to which the Reserved Matters/other details application relates was granted by the Mayor of London in March 2015. The outline application was submitted to the Council in April 2013. As the application was an application of potential strategic importance as defined in the Town and Country Planning (Mayor of London) Order 2008 it was, in the usual way, notified to the Mayor of London in accordance with the 2008 Order.
- 2.2 In October 2013 and before the Council had formally considered the application, the applicant asked the Mayor to exercise his statutory powers to 'call in' in the application for his own determination. The Council made representations to the Mayor opposing such a move, the Mayor of London nevertheless decided that he would determine the application.
- 2.3 The Council also made representations objecting to the application on the basis of inappropriate scale and massing and relationship with historic buildings, failure to link with Sayes Court and to accommodate The Lenox, limited scope for evolution of the scheme, various transport issues and uncertainty over community benefits. It recommended to the Mayor that the application be refused. Following a representations hearing, the Mayor resolved that permission be granted subject to satisfactory completion of a Section 106 Agreement. The Section 106 Agreement (to which the Council is a party) was concluded on 10 March 2015, and outline planning permission (**OPP**) was granted by the Mayor on the same date.
- 2.4 The Mayor also directed that the Council should determine the Reserved Matters applications and also discharge the conditions under the OPP.
- 2.5 The OPP permits the demolition of all non-listed structures at the site, and comprehensive redevelopment (to include retention and refurbishment of the Grade II Listed Olympia Building) to provide up to 419,100m² of mixed use development comprising up to:
- 321,000m² residential (Class C3) (up to 3,500 units);
 - 15,500m² business space (Class B1/live/work units);
 - 2,200m² for up to three energy centres;

- 32,200m² working wharf and vessel moorings (Class B2 and sui generis);
- 27,070m² hotel (Class C1);
- 5,810m² retail, financial and professional services (Classes A1 and A2);
- 4,520m² restaurant/cafes and drinking establishments (Classes A3 and A4);
- 13,000m² community/non-residential institutions (Class D1 and D2),
- 1,840 car parking spaces, together with vehicular access and a river bus facility.

2.6 The development is divided into 22 separate plots and is to be developed in 3 phases. Each plot is defined by a set of parameters (described in further detail in the assessment below) that fix its location within the site and its shape, the maximum and minimum height, width and length of each building within the plot and the extent of podiums. The parameters also fix road widths. The 22 development plots, 3 phases and safeguarded wharf are indicated in image 1 below:

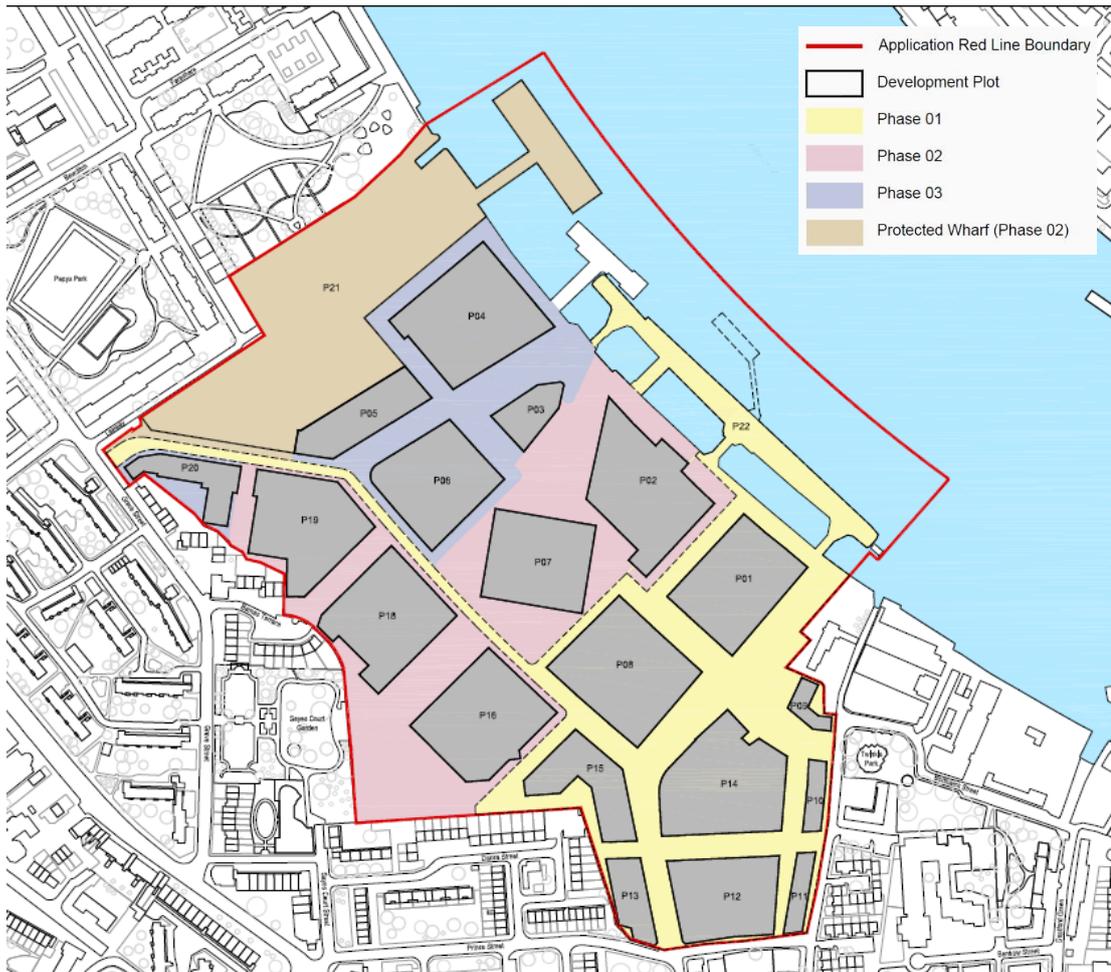


Image 1: Convoys Wharf Outline Plot and Phasing Plan (as amended by DC/18/107740)

2.7 The development has an anticipated 10-15 year build out programme.

2.8 The existing Section 106 Agreement includes the following (this is not an exhaustive list):

Community Infrastructure and Projects

- Primary school - delivery of a 2-Form entry primary school, with an option for increased capacity to 3-Form entry;
- Secondary and post sixteen education - £440,000 (up to £881,000 subject to viability);
- Local open space - £560,000;
- Local heritage and public art - £300,000;

- Community Trust - £250,000;
- Community projects (Lennox and John Evelyn Centre – subject to business plans) - £250,000;
- Feasibility study for the Lennox Project - £20,000;
- Healthcare Facility (subject to a lease with a Healthcare provider - £643,724 in lieu);

Housing

- Delivery of at least 15% affordable housing and a review mechanism
 - At not less than 30% Affordable Rent Dwellings;
 - At not less than 70% Intermediate Dwellings
 - Wheelchair Housing

Employment

- Wharf infrastructure and activation;
- Local employment and training initiatives (including the affordable business space at subsidised rents);
- Employment and Training Contribution - £500,000;

Transport

- Contributions towards highways works to Deptford High Street, Prince Street, Grove Street, Evelyn Street, Oxestalls Road, Deptford Church Street/ Deptford Broadway Junction and other highways in the vicinity - £1,417,500
- Further s278 Highway works to New King Street (widening and public realm improvements) and to northern section of Deptford High Street between Deptford Station and the Evelyn Street/New King Street;
- Pedestrian and cyclists improvements to Deptford Church Street/A2 junction;
- Delivery of river pier for timetabled passenger services and associated land facilities and financial contribution to Riverbus service - £3,000,000;
- New and diverted bus service (plus capacity enhancements to existing services on Evelyn Street) - £5,750,000;
- New and enhanced off-site bus stops - £99,500;
- Travel Plan for each use (including Travel Plan measures, car club spaces);
- Provision of Controlled Parking Zone - £250,000;
- Air Quality Monitoring - £100,000;
- Delivery of on-site spine road, Thames Path extension and a network of public pedestrian and cycle links within the site;
- Safeguarding of sites for two cycle hire docking stations;
- Monitoring costs - £400,000

Other matters

- Provision of Design and Access Panel to assist the submission of Reserved Matters Applications;
- Provision of Cultural Steering Group;
- Olympia Building Strategy
- Energy strategy (including prioritisation of SELCHP connection);
- CCTV scheme;
- Telecommunications monitoring and mitigation
- Wharf activation provisions.

3.0 SITE DESCRIPTION

- 3.1 The wider Convoys Wharf site is approximately 16.6 hectares (41.2 acres), representing about 50% of Lewisham's River Thames frontage. The majority of the eastern side of the application site forms the administrative boundary with the London Borough of Greenwich. The remainder is formed by the boundary with the Shipwright's Palace (listed Grade II*) which is within the Borough. The surrounding area is predominantly residential with the Pepys Estate and Pepys Park to the west and the Sayes Court Estate to the south. The Pepys Estate, including Aragon Tower, ranges from 3 storeys to 8 storeys with three tall buildings; two at 24 storeys and Aragon Tower at 30 storeys. The Sayes Court Estate is predominantly 3 to 5 storeys with some 11 storey blocks. The site is bounded by Leeway to the north west, properties on Grove Street/Prince Street, Barnes Terrace and Dacca Street to the south and Watergate Street to the east with properties ranging from 2 to 5 storeys.
- 3.2 Existing access to the site is via an entrance at the junction of Prince Street and New King Street. Evelyn Street (A200) and the northern end of Deptford High Street are approximately 100m to the south. Cycle Super Highway 4 is proposed along Evelyn Street in the to commence in June 2020 and be completed by Summer 2021. In terms of public transport services in the area, a number of bus services (47, 188, 199, N1, N47) run along Evelyn Street and one service (199) is routed along Grove Street (although not adjacent to the site). The nearest mainline stations are at Deptford and Greenwich (services to/from Cannon Street and Charing Cross via London Bridge), DLR services are at Greenwich Cutty Sark and Deptford Bridge, Underground services at Canada Water and Surrey Quays and Overground at Surrey Quays.
- 3.3 Approximately 9 hectares of the site is a protected wharf. The wharf is not currently operational. It is subject to a Safeguarding Direction issued by the Secretary of State in June 2000 which requires the Mayor to be consulted prior to the grant of planning permission relating to the area protected. In January, the Mayor approved the final recommendations of the review for submission to the Secretary of State for Housing Communities and Local Government. This recommends that the safeguarding be retained for Convoys Wharf with the boundary of the protected wharf amended to reflect the boundary of the OPP.
- 3.4 The site has a substantial and significant history having been the site of the Royal Dockyard since the 16th century and also the location of Sayes Court Garden and house, once occupied by John Evelyn. This history is visible with the Grade II listed building within the protected wharf area, Olympia Warehouse, constructed as cover to Slipways nos. 2 & 3 in the former Deptford Royal Dockyard. Gate posts at the junction of Grove Street and Leeway and the river wall are also listed Grade II. Other historic features on the site are archaeological remains which include the site of a Tudor Store House (a Scheduled Ancient Monument), a basin to the front of the Olympia Warehouse, the double dry dock and Sayes Court House. English Heritage (now Heritage England) identified Convoys Wharf as an Area of Archaeological Priority where significant buried remains of the former Royal Dockyard are likely to exist. Recent archaeological investigations have shown a number of that a number of archaeological features survive below ground.
- 3.5 A group of mature trees on the northeast corner of the site adjacent to the Shipwright's Palace (which lies outside the application site boundary) are subject to a Tree Preservation Order, as are trees located along the south-western boundary of the site.
- 3.6 The north-west corner of the Convoys Wharf site sits within the protected viewing corridor of St Paul's Cathedral from Greenwich Park and the wider setting consultation area in the foreground and middle ground.
- 3.7 Up until recently, there were 33 buildings on the site which were of late 20th century construction, save for the Olympia Warehouse which dates from 1846. In early 2011, a

number of the modern warehouse buildings were demolished. There are now 5 buildings retained on site, including the listed Olympia Warehouse.

- 3.8 The site has a public transport accessibility level (PTAL) spanning across 1a, 2, and 3.
- 3.9 The site is within the Deptford Creek/Greenwich Riverside Opportunity Area as identified in the London Plan. Convoys Wharf is designated as a Strategic Site within the Core Strategy and is located within the Deptford Regeneration and Growth Area.
- 3.10 Directly to the west of Convoys Wharf is the Oxestalls Road Strategic Site (also known as The Wharves, Deptford) which has planning permission for 1132 new dwellings in buildings ranging from 4-24 storeys. Phase 1 is under construction. Further west is the Plough Way Strategic site which is formed of four plots; Marine Wharf West, Marine Wharf East, Cannon Wharf and sites in Yeoman Street. All have planning permission with the total number of 1244 approved units. The Plough Way sites are now complete.

4.0 RELEVANT PLANNING HISTORY

Background

- 4.1 The site has relatively a limited formal planning history but as set out above, has a long and significant history as a naval dockyard dating from the 17th century which has left an important legacy in the form of archaeological remains on and adjacent to the site. The site was used by Convoys, a subsidiary company of News International Plc, for the importation and transshipment of newspaper products up until September 1999 when Convoys operations were relocated to Medway. Parts the site were then used for storage purposes but the site has been vacant since 2010 and various modern buildings have been demolished.
- 4.2 In 2002 News International submitted an outline application for the comprehensive residential-led mixed use redevelopment of the site for a total of 447,045m² of floorspace providing c. 3,500 with employment, leisure and retail uses. The Council resolved to grant planning permission subject to completion of a S106 agreement, but due to a number of concerns raised by the GLA, principally focused on the protected wharf, affordable housing and transport, the referral was withdrawn at the request of the GLA.
- 4.3 The site was subsequently acquired by the current owners, Hutchinson Whampoa (**HW**), and the planning application was amended but ultimately withdrawn when HW engaged new masterplanners, Farrells and submitted a new outline planning application which led to the grant of the OPP by the Mayor in March 2015.

Other Relevant Applications

- 4.4 An amended phasing plan (Condition 22) was approved on 27th June 2018 as per Image 1 above (planning application reference number DC/18/107740).
- 4.5 DC/19/113231 - An application submitted under Section 96A of the Town and Country Planning Act 1990 for a Non-material amendment in connection with the Planning Permission DC/13/83358 approved (GLA reference D&P/0051c/GC/18) 10th March 2015 for the comprehensive redevelopment of Convoys Wharf to provide a mixed-use development of up to 419,100m² comprising:
- up to 321,000m² residential floorspace (up to 3,500 units) (Use Class C3)
 - up to 15,500m² employment floorspace (Class B1/Live/Work units) including up to 2,200m² for 3 no. potential energy centres
 - wharf with associated vessel moorings and up to 32,200m² of employment floorspace (Sui Generis & Class B2)

- up to 5,810m² of retail and financial and professional services floorspace (Classes A1 & A2)
- up to 4,520m² of restaurant/cafe and drinking establishment floorspace (Classes A3 & A4)
- up to 13,000m² of community/non-residential institution floorspace (Class D1) and assembly and leisure (Class D2)
- up to 27,070m² of hotel floorspace (Class C1)
- river bus jetty and associated structures
- 1,840 car parking spaces together with vehicular access from New King Street and Grove Street
- retention and refurbishment of the Olympia Building and demolition of all remaining non-listed structures on site

In order to allow an amendment to minimum development parameters in relation to P08 and the minimum and maximum development parameters in relation to P15.

- 4.6 Reserved Matters Applications for Plot 15 (DC/19/111912) and Plot 22 (DC/18/107620) and discharge of/approval under conditions in relation to those Plots have also been received by the Council. These applications are the subject of separate reports which are also on the same agenda as the application in relation to Plot 8.
- 4.7 A number of further applications have been submitted and approved in relation to advance site works and other pre-commencement conditions as follows:
- 4.8 DC/15/094797 - Partial details for the advanced site works phase relating to the haul road submitted in partial compliance with Condition (45) (i) (a) and (b) Contamination Studies of the planning permission DC/13/83358 dated 10th March 2015 – Approved 19 February 2016
- 4.9 DC/15/094799 - Details related to the advanced site works phase submitted in partial compliance with Condition 47 Surface Water Control Measures of planning permission DC/13/83358 dated 10th March 2015 – Approved 5 February 2016
- 4.10 DC/15/094800 - Details for the advanced works phase submitted in partial compliance with Conditions (34, 35, 36, 37, 38 and 39) Archaeological Work of the planning permission DC/13/83358 dated 10th March 2015 – Approved 24 February 2016
- 4.11 DC/16/095903 - Details submitted in compliance with Condition (44)(i) Site-Wide Construction Code of Practice of planning permission DC/13/83358 dated 10th March 2015 – Approved 21 April 2016
- 4.12 DC/16/096970 - Details submitted in compliance with Condition (44)(ii) Phase-Specific Construction Code of Practice of planning permission DC/13/83358 dated 10th March 2015 – Approved 01 June 2016
- 4.13 DC/17/100954 - Details submitted in compliance with Condition 6 (River Wall Surveys) of the planning permission DC/13/83358 dated 10th March 2015 – Approved 21st June 2018
- 4.14 DC/17/104961 - Details submitted in compliance with Condition 41 (Ecological Management Strategy) of the planning permission DC/13/83358 dated 10th March 2015 – Approved 23rd March 2018

5.0 THE PROPOSALS AND SCOPE OF ASSESSMENT

- 5.1 This current application seeks approval of Reserved Matters for Plot 08, together with the approval of other details under Conditions 20 and 21 of the OPP and approval/discharge under/of the conditions listed in the Table at paragraph 5.7 below.

5.2 The comprehensive redevelopment of the site has already been assessed and the OPP granted based on a number of development principles and parameters. These include the overall quantum of development and mix of uses, the scale, height and massing of buildings, as well as the general layout of the site including the location of buildings, routes and open spaces.

5.3 Condition 20 of the OPP is set out below. The 'Reserved Matters' required to be approved are the details referred to as layout (20(i)(a)), scale (20(i)(b)), appearance (20(i)(c)), access (20(i)(e)) and landscaping (20(i)(f)). The Town and Country Planning (Development Management Procedure) (England) Order 2015 defines the reserved matters as:

- (i) layout: the way in which buildings, routes and open spaces within the development are provided, situated and orientated in relation to each other and to buildings and spaces outside the development;
- (ii) scale - the height, width and length of each building proposed within the development in relation to its surroundings;
- (iii) appearance - the aspects of a building or place within the development which determines the visual impression the building or place makes, including the external built form of the development, its architecture, materials, decoration, lighting, colour and texture;
- (iv) landscaping - the treatment of land (other than buildings) for the purpose of enhancing or protecting the amenities of the site and the area in which it is situated and includes
 - (a) screening by fences, walls or other means;
 - (b) the planting of trees, hedges, shrubs or grass;
 - (c) the formation of banks, terraces or other earthworks;
 - (d) the laying out or provision of gardens, courts, squares, water features, sculpture or public art; and
 - (e) the provision of other amenity features;
- (v) access: the accessibility to and within the site, for vehicles, cycles and pedestrians in terms of the positioning and treatment of access and circulation routes and how these fit into the surrounding access network.

5.4 An application for approval of Reserved Matters is not an application for planning permission. In terms of formal requirements, the Town and Country Planning (Development Management Procedure) (England) Order 2015 states only that applications for approval of reserved matters "must include such particulars, and be accompanied by such plans and drawings, as are necessary to deal with the matters reserved in the outline planning permission".

5.5 It is important to note that as OPP has been granted, the principle of the development and those elements of the development that have already been approved in outline (including the road layout, the overall quantum and mix of uses, the scale, height and massing of buildings and the general layout of the site as identified on the approved parameter plans) do not form part of the current application and are not matters for reconsideration as part of the determination of the proposed reserved matters or other matters submitted for discharge/approval under conditions.

5.6 Condition 20 provides as follows:

Reserved Matters/ approval of details

- i) Development other than works approved under Condition 21 shall not commence in a Phase or Sub-Phase or Plot approved under Condition 22 until layouts, plans,

sections, elevations and other supporting material for that Phase. Sub-Phase or Plot detailing:

- a) Siting and layout of the buildings and other structures;
 - b) Scale and design of the buildings (including floor areas, height and massing);
 - c) External appearance (including samples of the materials and finishes to be used for all external surfaces and including but not limited to roofs, elevation treatment and glazing);
 - d) Measures to appropriately mitigate any potential overlooking issues (including details of proposed privacy screening);
 - e) Means of access (and details of surface treatments) for carriageways, cycleways, footways, footpaths and pedestrian access routes (identifying those which are to be publicly accessible) and routes to/from car parking and cycle storage/parking;
 - f) Hard and soft landscaping and planting, site boundary treatments of all publicly accessible open space and all private open space (including play space, private residential amenity space and communal residential amenity space); and,
 - g) Impact study of the existing water supply infrastructure (to determine the magnitude of any new additional capacity required in the system and a suitable connection point – for approval by the Local Planning Authority in consultation with Thames Water) have been submitted to and approved in writing by the Local Planning Authority.
- ii) The details of publicly accessible routes required to be submitted pursuant to part (i)(e) of this Condition shall include timescales for completion of such publicly accessible routes by reference to the occupation of residential units within the Phase, Sub-Phase or Plot in which they are to be provided.
- iii) The development shall in all aspects be carried out in strict accordance with the details approved under this Condition.
- iv) Not more than the relevant threshold of residential units as specified in the details approved pursuant to part (ii) of this Condition shall be occupied until the publicly accessible routes have been completed in strict accordance with the details approved pursuant to part (i) of this Condition.

Discharge of Conditions

5.7 In addition to the application for the approval of Reserved Matters for Plot 08, the applicant has also applied for approval of the other details required by Condition 20 so far as relevant to Plot 08 and to discharge certain other conditions of the OPP. The relevant conditions are outlined listed below in Table 1. The full wording of each of the conditions can be viewed in the OPP, a copy of which is attached to this Report as Appendix 1.

Number	Title
3(ii)	Microclimate: wind
7	Building design Statement and Tall Buildings Design Statement
8	Reconciliation Statement
10	Housing (Minimum residential space standards)
12(i)	Lighting
13	Heritage Statement
14	Biodiversity
15	Energy Statement
19	Drainage and flood risk
21	Infrastructure and other details
33	Details of cycle parking
45	Contaminated Land

47	Surface water control measures
50(i)	Electric vehicle charging points

Table 1: Conditions sought for discharge

5.8 The details considered below in relation to the Reserved Matters are also material to consideration of other matters required to be approved under Condition 20. The assessment of layout is also relevant to siting (part of 20(i)(a)), the assessments of scale and appearance are also relevant to design (part of 20(i)(b)). The assessment of playspace (part of 20(i)(f)) is also considered under landscaping.

Overview of Plot 08 Proposals

5.9 In accordance with the approved Development Specification (CW05A), the key components of Plot 08 are as follows:

- 37,400 sqm of residential (Class C3) floor space
- 1,450 sqm of shops (Class A1) and financial and professional services (Class A2) floor space; and
- 200 sqm of restaurant and cafes (Class A3) and drinking establishments (Class A4) floor space;

5.10 The approved phasing programme (DC/15/094795 as amended by DC/18/107740) indicates that the works to Plot 08 are to be delivered as part of Phase 1.

5.11 As approved by the OPP, the development plot lies within the Eastern Gateway character area, which would consist of a series of predominantly residential buildings while providing a link from Deptford High Street, along New King Street to the River Thames. Commercial uses would be provided at ground floor level, fronting onto the new streets formed. The location of Plot 08 is indicated by Image 2 below.

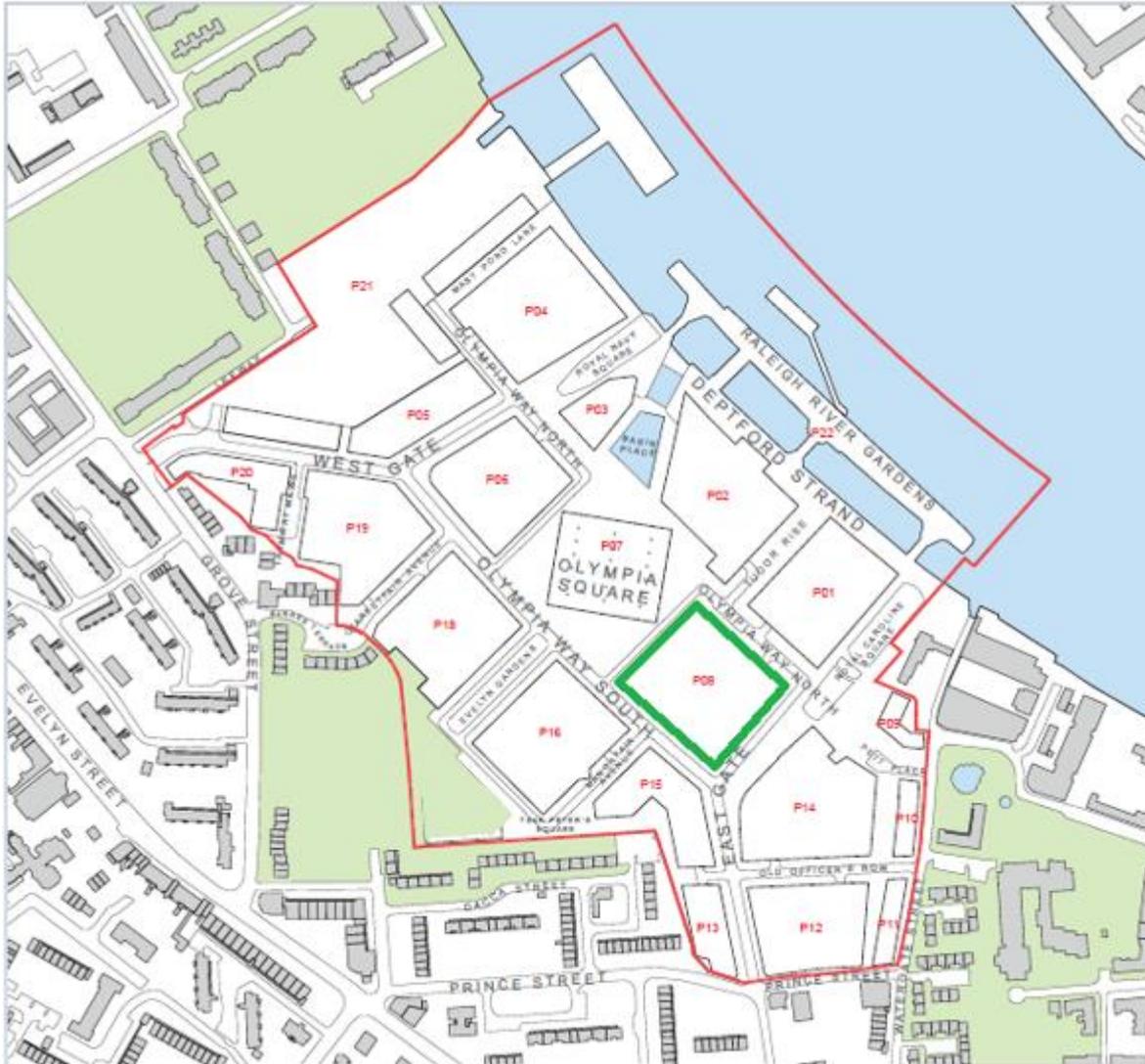


Image 2: Plot 08 (outlined in green) in relation to surrounding development plots

- 5.12 This Reserved Matters Application proposes the construction of “U” shaped perimeter block typology building with a two storey central podium providing shared amenity space for the occupants of P08, surrounded by blocks ranging from 10 to 12 to 14 storeys in height. The proposed heights would range from 11.9m at podium level to 39.9m at 11 storeys to 52.6m at 15 levels of accommodation (14 storeys).
- 5.13 At ground level 1,450sqm of A1/A2 use and 200sqm of A3/A4 use is proposed. At upper levels, the scheme proposes a total of 456 units equating to 275 1B/2P units and 181 2B/4P.
- 5.14 Vehicular access to the site would be provided from New King Street and onto the new access road, with access to the secure car park and servicing on the north-eastern frontage. The Development proposes space for 182 residential car parking spaces and 14 non-residential spaces. There would also be provision for electric vehicle charging points detailed further below.
- 5.15 816 cycle parking spaces are proposed as part of the scheme as follows:
- 776 internal long-stay spaces for residents including;
 - 40 accessible Sheffield stand bays
 - 170 standard Sheffield stand bays
 - 566 two tier stand bays
 - Eight internal short-stay spaces for visitors/residents;

- Four external short-stay spaces for visitors/residents;
- 28 external short-stay spaces for customers

6.0 CONSULTATION

6.1 This section outlines the consultation carried out by the Applicant prior to submission of the current application and the Council following the submission of the application, and summarises the responses received.

Applicant's Consultation

6.2 The applicant engaged the public through a public exhibition which was held at Deptford Methodist Church on Creek Street. Invitations for the exhibitions were sent to local residents around the site, both within the Borough and the London Borough of Greenwich. The applicant has indicated that invitations were sent out to invite local residents and businesses to the public exhibition. The applicant has stated that a further 19 letters were also sent out to local stakeholders and community groups

6.3 The exhibition was held on 7 and 8 July 2017. The exhibition saw 117 visitors attend over the course of the two days. The applicant has stated 50 feedback forms were returned.

6.4 The applicant held two further drop-in consultation events on Saturday 29th February 2020 and Tuesday 3rd March 2020 at the Community Action Centre at Grove Street.

Council's Application Consultation

6.5 The Town and Country Planning (Development Management Procedure) (England) Order 2015 does not prescribe minimum consultation requirements for applications for approval of Reserved Matters or under conditions, nor does the Council's adopted Statement of Community Involvement. However, in common with previous applications and to ensure that statutory and non-statutory consultees as well as members of the public and other interested parties were made aware of the current application, the approach to public consultation for applications for planning permission was adopted. A letter drop was carried out to 1,351 homes and businesses in the area surrounding the application site, an advert was also placed in the Local Press and seven public notices were displayed around the site.

6.6 Emails providing a link to the application were sent to the relevant ward Councillors.

6.7 The following statutory consultees and stakeholders were also consulted:

- Docklands Light Railway
- Environment Agency
- Greater London Authority
- Historic England
- Highways England
- London Borough of Tower Hamlets
- London Borough of Southwark
- London City Airport
- London Fire and Emergency Authority
- Metropolitan Police Designing Out Crime Officer (Lewisham)
- Museum of London
- National Grid
- Natural England
- Network Rail
- Port of London Authority

- Royal Borough of Greenwich
- Thames Water
- Transport for London

6.8 The following local groups were consulted:

- Creekside Education Trust
- Creekside Forum
- Deptford Folk
- Deptford High Street Association
- Deptford Neighbourhood Action
- Friends of the Earth
- Lewisham Cyclists
- Lewisham Street Traders Association
- London Wildlife Trust
- Greater London Industrial Archaeology Society
- Greenwich Conservation Group
- Greenwich Society
- Naval Dockyards Society
- Pepys Community Forum
- Royal Parks Agency
- The Victorian Society
- Voice4Deptford

6.9 The following Council services were consulted:

- Drainage and Flood Risk
- Ecological Regeneration
- Education
- Environmental Protection
- Highways
- Housing Strategy
- Parks

6.10 In addition, the application has been advertised and consulted upon pursuant to the Town and Country Planning (Development Management Procedure) Order 2010.

6.11 The objections received and Officer responses with cross-references to where they are addressed within this Report are set out below. It is relevant to note that a number of the objections raise matters that relate to principles of the development that have previously been approved under the OPP. Whilst these are reported so that Members are aware of the range of comments received, they are not material considerations in the determination of this current application. Accordingly, weight should only be given to those comments which relate to the Reserved Matters as defined or otherwise relate to the subject matter of the conditions under consideration.

6.12 Following the initial consultation, the Council carried out a further reconsultation in February 2020 where another letter drop was carried out to 1,351 homes and businesses in the area surrounding the application site and an advert was also placed in the Local Press.

Written responses from Ward Councillors and Members of Parliament

6.13 Objections have been received from the Evelyn Ward Councillors and Vicky Foxcroft MP. A comment from a Tower Hamlets MP was received stating that residents in Tower Hamlets should be consulted – officers confirmed that this has occurred. The objections from the Evelyn Ward Councillors are summarised in Table 2 below:

Summary of Representations	Officer Response (paragraph)
The proposed design does not complement or acknowledge the rich heritage of the site	8.46-8.56, 8.98-8.151, 8.119-8.135
No social homes are proposed for P08	8.17-8.18
No family homes are proposed for P08	8.17-8.18
The submission fails to take account of air quality	8.297-8.301
No additional provision for outdoor play areas for the children of residents. Little outdoor play space, none of which is suitable for children over 5.	8.91-8.97
The consultation carried out by the applicant is completely inadequate, particularly when compared to other schemes	6.2-6.18

Table 2: Summary and officer response to Evelyn Ward Councillor's representations

6.14 The objections from Vicky Foxcroft MP are summarised in Table 3 below:

Summary of Representations	Officer Response (paragraph)
Block 08 is comprised entirely of private accommodation, none larger than two bedrooms. Deptford residents need socially rented, family sized properties of three beds or more.	8.17-8.18
In their Core Strategy Policy 1, the London Borough of Lewisham committed to seek 50% affordable housing in all new developments. Affordable housing in this context would be split at 70% social rented and 30% intermediate housing and yet no socially rented homes are planned at Block 08.	8.17-8.18
There has been no financial transparency on behalf of the applicant	See Executive Summary
The cultural significance of the application site has been ignored by the current plans	8.98-8.129
The proposed layout of the units is to the detriment of young children	8.91-8.97
The applicant provides no outdoor playspace for the children of residents	8.91-8.97

Table 3: Summary and officer response to Vicky Foxcroft MP's representations

Written Responses received from Local Residents and Organisations

6.15 As a result of the public consultation process, 81 representations were received objecting to the proposed development, these included objections from Alliance for Childhood, Deptford Folk, Pepys Community Forum and Voice4Deptford. A summary of the representations is outlined in Table 4 below.

6.16 The representations from community groups and the public are summarised as follows:

Summary of Representations	Officer Response (paragraph)
<i>Design and Appearance</i>	
The building fails to take advantage of surrounding architectural design	8.119-8.135
The mixed-use development does not correspond to the history of the site	8.30-8.168
The design for P08 is three years old and is outdated	8.119-8.135

The design of P08 blocks the creation of a straight line of travel to the river	8.31
Plot 08 needs to have much more vegetation to comply with draft London Plan Policy G5 “Urban Greening” as well as climate change policies	8.136-8.145
The building should be context-sensitive and site specific with much more green vegetation and landscape all of which should reflect its historic significance.	8.119-8.135
The brick facade does not represent the traditional style of brickwork. The use of brickwork in the facade could bring a playful and artistic sense of heritage and quality design	8.48-8.59
The building in Plot 08 will be brick clad yet lacks the character of maritime Deptford. It has the ‘anywhere’ appearance of an international style. More attention should be paid to ways to make it part of the Deptford style.	8.48-8.59
The colonnade is nondescript and the plan in the design process which has arches rather than lintels makes a stronger reference to the built heritage of the neighbourhood compared with what is being proposed.	8.57
The balconies are thoughtless and boring	8.48-8.59
The area should be a community skypark for residents	8.91-8.97, 8.83
Plot 08, is awkwardly angled both relative to the Grade II listed Olympia Warehouse and blocks the long vista of the ‘arrow head’ from Deptford High Street	8.26-8.34
The Grade II Listed Olympia warehouse – which is and should be centre stage – is being severely marginalized by surrounding very tall towers and high/wide buildings	8.26-8.34
<i>Consultation</i>	
There has not been extensive consultation in relation to proposals for Plot 08	6.2-6.18
The application should not be considered until adequate consultation has taken place	6.2-6.18
<i>Playspace and Young People</i>	
There is no intervisibility between young parents and children provided by the playspace proposed	8.91-8.97
Play provision on Convoys Wharf is inadequate and there has not been sufficient investment locally to provide opportunities for play offsite. There should be appropriate provision for different age groups, including older children and teenagers within 400 metres of the development and be accessible via a safe route from children’s homes	8.91-8.97
Young people need to be involved in the design process	8.91-8.97
There is a lack of communal play space for older children	2.8, 8.91-8.97

<i>Accommodation</i>	
85% of the development would be luxurious residential	8.17-8.19
Plot 08 proposes no social housing	8.17-8.19
Plot 08 does not provide any affordable family accommodation.	8.12
Plot 08 does not propose the correct tenure mix to promote mixed and balanced communities in the borough	8.13-8.19
Plot 08 does not provide a mix of unit sizes appropriate to the location.	8.14
The flats appear cramped with inefficient floor layouts, built to the minimum space standards.	8.20
A viability argument should not be accepted as a reason for not providing more affordable housing	See Executive Summary
The Convoys Wharf Outline Planning permission offers, overall, 85% private luxury apartments and just 15% 'affordable', that is only 500 out of the 3,500 units	8.17-8.19
<i>Infrastructure and Highways</i>	
There is a shortage of school places in the area	See Executive Summary
There is not adequate GP facilities in the area	See Executive Summary
The site does not have an appropriate PTAL for the scale proposed	2.5
All reserved matters applications for Convoys Wharf must demonstrate the application of the Healthy Streets approach (TfL Policy).	8.240-8.247
There must be a timescale for the introduction of publicly accessible riverboat services. The agreed outline permission states that "Delivery of the facility would be the subject of 3rd party agreement". Lewisham Council needs to liaise with the GLA and TfL to ensure this happens at the earliest possible opportunity.	Required to be delivered prior to occupation of 750 residential units
<i>Environment</i>	
Why has no space been allocated to biodiversity or growing of food?	8.8-8.11
<i>Other</i>	
The outline planning permission has timed out	See Executive Summary
A new more imaginative cultural strategy is required and the applicant has failed to meet their requirements outlined in the S106 in this regard	See Executive Summary
The Cultural Steering Group has been non-functional	See Executive Summary
Lack of financial viability transparency	See Executive Summary
Disregard for fire regulations outlined in the applicant's design and access statement	8.314-8.319
Flood risk and sustainable drainage must be properly considered	8.167-8.175, 8.302-8.306
The units on the ground floor of Plot 08 are for business and amenity use. These should be fully fitted (not "Shell & Core") with a marketing	8.8-8.11

strategy in place. There must be a guarantee that these units will not be converted to residential use	
The development must ensure the provision and future management of free drinking water at appropriate locations in new or redeveloped public realm. Free drinking water fountains that can refill water bottles as well as be drunk from should be provided in appropriate locations in the public realm.	The applicant states that this will be addressed in future Reserved Matters applications in appropriate locations around the site

Table 4: Summary and officer response to representations received

- 6.17 Given the application received 81 representations objecting to the proposed development, a Local Meeting was carried out in accordance with the Council's Statement of Community Involvement.
- 6.18 Circa 35 people (excluding officers and representatives of the applicant) attended the Local Meeting, which was held at the Evelyn Community Centre, in close proximity to the application site, on 30th July 2019. The minutes of the local meeting are attached as Appendix 2.

Written Responses received from Statutory Agencies and Authorities

Docklands Light Railway

- 6.19 No response
- Environment Agency*
- 6.20 Flood risk
- 6.21 We note that the site is located in Flood Zone 3 which is defined as having a 'high probability' of river and sea flooding by the 'flood risk and coastal change' section of the national Planning Practice Guidance (PPG) (Table 1: flood zones of the flood zone and flood risk tables).
- 6.22 Please note that there may be other sources of flooding which affect this site – such as surface water and groundwater flooding – which are not within our direct remit, but nevertheless could be important considerations for managing flood risk for the proposed development. Indeed, consideration of other sources of flooding may be necessary to inform suitable mitigation measures to reduce the impact of any such flooding. Under the Flood & Water Management Act 2010, the Lead Local Flood Authority (LLFA) has the lead role in such flooding matters.
- 6.23 Flood mitigation measures
- 6.24 We strongly recommend that 'more vulnerable' residential accommodation, particularly sleeping accommodation, is set at the first floor level and above, or, if this approach is not possible, above the modelled flood level. We are pleased to note that no residential accommodation will be situated at the ground floor level within Plot 08, as indicated on the submitted ground, first and second floor plans by Farrells (dated February 2018 with references CON4-PA-05- 100, CON4-PA-05-101 and CON4-PA-05-102 respectively).
- 6.25 Groundwater and land contamination
- 6.26 As this is a phased development, the following comments relate to the discharge of planning conditions with respect to Plot 08 only:
- 6.27 Surface water drainage – condition 19

- 6.28 We have reviewed the submitted drainage strategy by Cundall Johnston & Partners LLP (dated 14 February 2018 with reference 1011666-RPT-C002, Revision C). As surface water will discharge to the surface water sewer and will not be discharged to ground, we have no concerns with the proposed surface water drainage strategy and have no further comments to make from a groundwater protection perspective.
- 6.29 Foundation design and groundworks – condition 38 (Planning Officers note that this is not included within the scope of the current application)
- 6.30 We have reviewed the submitted foundation method statement by AECOM (dated February 2018). As continuous flight auger (CFA) piling will be used, we have no concerns with the proposed foundation methodology.
- 6.31 Contaminated land (site investigation; remediation scheme) – condition 45(i)
- 6.32 We have reviewed the submitted remediation strategy by AECOM Infrastructure & Environment UK Ltd (dated 31 January 2018 with reference CW-PO8-ACE-1220- 011-E-01), in conjunction with the site-wide remediation strategy by AECOM Infrastructure & Environment UK Ltd (dated 22 October 2015 with reference CWCWM- ACE-RP-1220-001-E-04). We understand that a remediation strategy will be submitted for each plot of the phased development, with each remediation strategy being informed by relevant site investigation works. We therefore accept, and have no concerns with, the submitted remediation strategy for Plot 08. We look forward to receiving further remediation strategies for each plot of the phased development in due course.

Greater London Authority

- 6.33 Confirmed no objection.

Historic England (Designated Built Heritage Assets)

- 6.34 Confirmed no objection.

Historic England (Greater London Archaeology Advisory Service)

- 6.35 Initially raised objection with regard to detail provided in relation to archaeological conditions, the proposed remediation strategy and the applicant's response to Condition 13 of the Outline Planning Permission – this is outlined in detail in assessment below. Historic England are now satisfied that the proposed development suitably addresses Condition 13 of the OPP and their concerns regarding the remediation strategy. The archaeological conditions have been removed from the scope of this application

London Borough of Southwark

- 6.36 Raised several queries in relation to the Transport Statement. These were provided by officers and no further comment received.

London Borough of Tower Hamlets

- 6.37 No response

London City Airport

- 6.38 No response

London Fire and Emergency Authority

- 6.39 With reference to planning application DC/18/107698, requesting advice in respect of the abovementioned premises, please refer to the comments below.
- 6.40 Pump appliance access and water supplies for the fire service were not specifically addressed in the supplied documentation, however they do appear adequate. In other respects this proposal should conform to the requirements of part 85 of Approved Document B.
- Metropolitan Police Designing Out Crime Officer (Lewisham)*
- 6.41 No objection subject to Secured By Design condition
- Museum of London*
- 6.42 No response
- National Grid*
- 6.43 No response
- Natural England*
- 6.44 Natural England currently has no comment to make on the reserved matters pursuant to conditions.
- 6.45 Should the proposal be amended in a way which significantly affects its impact on the natural environment then, in accordance with Section 4 of the Natural Environment and Rural Communities Act 2006, Natural England should be consulted again. Before sending us any further consultations regarding this development, please assess whether the changes proposed will materially affect any of the advice we have previously offered. If they are unlikely to do so, please do not re-consult us.
- Network Rail*
- 6.46 No response
- Port of London Authority*
- 6.47 No response
- Royal Borough of Greenwich*
- 6.48 No response
- Thames Water*
- 6.49 Thames Water confirm they are happy for the foul water condition referenced, to be discharged based on the information submitted.
- 6.50 Thames Water confirm they are happy for the surface water condition referenced to be discharged based on the information submitted.
- 6.51 TW agree to discharge condition 20 in relation to water infrastructure.
- Transport for London*
- 6.52 Raised Car Parking

- 6.53 182 off-street residential parking bays are proposed which includes 46 accessible bays, 42 electric parking bays and 94 standard bays. The quantum of car parking proposed is within the London permitted levels and also within the levels agreed at the outline stage which is welcomed. TfL requests that additional information on car parking management be provided which includes a strategy for how the proposed provision will be allocated. For the residential spaces, consideration should be given to allocating the spaces to the larger units only. Information on how the 14 on-street spaces will be managed should also be included to prevent the misuse of the spaces.
- 6.54 Electric Parking Provision
- 6.55 The proposed provision of 23% active and up to 40% passive provision for electric vehicle charging is welcomed. The new draft London Plan requires 20% active provision and passive provision for all remaining spaces. We request the applicant considers increasing their passive provision to encompass all car parking spaces so that the site is future proofed for the use of electric vehicles and help meet the Mayor's target for carbon-free travel by 2050.
- 6.56 Car Club Parking Provision
- 6.57 The two proposed car club bays are welcomed and it is noted that there is the potential to increase this to four bays at a later stage to meet the levels of demand. It is not clear from the submitted drawings where the space for these additional two bays is being safeguarded and we request that the applicant submits additional information to clarify this.
- 6.58 Cycle Parking
- 6.59 The planning statement and transport statement to support the application states that the cycle provision will be the following:
- 844 in total broken down as follows:
 - 784 Internal long stay for residential
 - 8 Internal short stay for residential
 - 4 external short stay for residential
 - 28 external short stay for retail
 - 20 internal long stay for retail
- 6.60 This level of provision is acceptable and in line with the minimum standards of the draft London Plan. However the drawings included within the Design and Assess Statement and the Transport Statement all refer to a total cycle parking provision of 836 does not accurately reflect the 844 actual total proposed. TfL requests that the plans are updated so that all required cycle parking is provided.
- 6.61 Travel Plan
- 6.62 The submitted Transport Statement states that Site-wide and Phase specific Travel Plans will be submitted within 12 months of commencement which is in line with the Section 106 agreement from the outline scheme.
- 6.63 It is noted that the Framework Travel Plan proposed a target for at least 75% of construction workers to travel by sustainable modes, however the baseline targets in the FTP were all 'To be confirmed'. TfL requests if any further information on this can be provided by the applicant at this stage considering that construction workers will be on site before any further Travel Plans are produced for the site.
- 6.64 Delivery and Servicing

- 6.65 TfL requests that a Delivery and Servicing Plan (DSP) be produced in line with Policy T7 of the draft London Plan and TfL guidance. The DSP should identify efficiency and sustainability measures to be undertaken once developments are operational is submitted to and approved by London Borough of Lewisham in conjunction with TfL prior to occupation. The DSP should also include any relevant information on how the loading bays will be managed (it is noted that one of the bays is temporary) and if any side wide procurement strategies could be implemented e.g. commercial refuse. TfL further requests that the submission of the plans should be secured via appropriate planning conditions.
- 6.66 Construction Logistics Plan
- 6.67 TfL requests that a Construction Logistics Plan (CLP), as referred to in the draft London Plan Policy T7, which identifies efficiency and sustainability measures to be undertaken while developments are being built is submitted to and approved by London Borough of Lewisham in conjunction with TfL before construction work commences on site. TfL further requests that the submission of the plans should be secured via appropriate planning conditions.
- 6.68 Cycle Hire
- 6.69 Condition 18 of the outline consent states that details of areas at the site to be safeguarded for cycle hire docking stations be submitted with the first RMA for phase 1. It is noted in the submitted material that this will be done as part of the second RMA application for Plot 01 which is targeted for 2019. TfL are supportive of this and request the appropriate planning conditions are amended to reflect this requirement.
- 6.70 In conclusion, TfL requests additional information is provided as outlined above prior to being supportive of the application.

Responses from Council Departments

Drainage and Flood Risk

- 6.71 Initial objections raised as follows:
- The applicant has not stated the greenfield runoff so we are unable to assess the runoff proposals against the requirements of the London Plan.
 - There are two different proposed runoff rates stated within the documentation – 5.92 l/s and 46 l/s. This will need to be clarified and then assessed against the greenfield runoff rate.
 - There have been no calculations submitted which outline how much storage is required in order to achieve the proposed runoff rate.
 - The drainage strategy is unclear – for example, how is the storage at podium level achieved.
 - According to the London Plan, there should be no surcharging during the 30yr event, while the applicant has designed the system to possibly surcharge but not flood during the 30yr event.
 - We cannot find the cited AECOM's drainage strategy document for the Convoys Wharf drainage infrastructure which is described as containing information in terms of attenuation and discharge rates and a maintenance plan.
- 6.72 Please can the applicant submit information which:
- Informs on what the greenfield runoff rate for the site is
 - Clarifies what the proposed discharge rate is
 - Provides calculations which support the storage requirements in order to achieve the proposed runoff rate

- Clearly sets out the proposed drainage strategy and how these features will work
- Addresses the London Plan's policies on on-site flood risk in terms of surcharging water.
- Provides the cited Drainage Strategy (AECOM) for reference.

Ecological Regeneration

- 6.73 Please be mindful that the lighting strategy and reasoning for a dark corridor was to enable bats to travel north to south and connect with the Thames. I concede that this can be a challenge for the developer but it should not be forgotten and we should not collude with the assertion that the dark corridor is the Thames itself as has been implied in the lighting strategy.
- 6.74 Please can we therefore seek assurances that the design principle of a north south corridor is acknowledged and explored.

Education

- 6.75 No response

Environmental Protection

- 6.76 Request plot specific land contamination documents as required by condition 45.

Highways

- 6.77 I have reviewed the Plot 08 Transport Statement Addendum that was submitted in response to my initial comments on the application, and can confirm based on the additional details provided the proposal is unobjectionable subject to the following:
- 6.78 A phase-specific Code of Construction Practice is required prior to commencement of works at the site. The document should include a commitment to participate in the Evelyn Street Corridor Construction Logistics Plan Forum.
- 6.79 Cycle parking details are required, cycle parking at the site should be in accordance with London Cycle Design Standards.
- 6.80 A Car Parking management Plan should be provided prior to occupation, it should include details of how off-street parking (within the block) will be allocated, and how on-street parking (including the proposed loading bays, the turning head & car club bays) and any informal parking in the public realm will be enforced/managed.
- 6.81 The plan should include details of how access to the car park will be controlled (i.e. barrier or fob system).
- 6.82 Details of the proposed Electric charging points should be provided (in accordance with London Plan standards).
- 6.83 A Delivery & Servicing Plan should be provided prior to occupation, it should include details of facilities (such as concierge services, caretaker services or holding areas) which could minimise the impact of delivery & servicing activities at the site. A Waste management strategy should be included within the Delivery & Servicing Plan, it should identify collection areas within 10m drag distance of the carriageway.
- 6.84 The proposed access to the site for Plot 08 will be via Prince Street /New King Street, and it is proposed that the access via Grove Street would only be used as an emergency access. To facilitate/enable service and emergency vehicle access to the site (as illustrated in the swept path analysis drawings submitted within Appendix B Transport Statement

Addendum), the applicant is required enter into a S278 Agreement with the Highway Authority to secure improvement works to the Prince Street /New King Street junction and waiting restrictions along the service vehicle route between the application site and Evelyn Street.

- 6.85 An assessment of pedestrian comfort levels was also undertaken in the Transport Statement Addendum to determine if the proposed width of internal footways are appropriate. The assessment concluded the proposed footways are of adequate width to accommodate all expected future pedestrian demand, for Plot 08 and the wider consented scheme. However, the assessment did identify (In section 8.8) that the “proposed footway which runs along the north-eastern side of Plot 08 typically varies between 2.4-3.5m in width at the proposed locations of on-street parking bays, lighting columns and cycle parking, and the narrowest section (2.4m) is situated between a lighting column and the proposed building edge”. Given the density of the development and the predicted footfall within the site, to future-proof the development it would be desirable for all footways to be a minimum of 3m wide.
- 6.86 In addition, the Transport Statement Addendum (Section 8.10) also confirms the “ footways on the eastern and western sides of the spine road typically vary between 2.7-3.1m in width, reducing to 2.0-2.3m in width at the proposed locations of lighting columns”. However, the spine road will be used by a bus route and have bus stops in the future , as a result it would be desirable for the footways to be a minimum of 3m wide on the spine road.
- 6.87 A high quality pedestrian/cycle environment is required to encourage the occupiers to use sustainable modes of travel to from the site, and the Transport Statement doesn’t assess the pedestrian/cycle routes between the site and public transport modes. So, a Healthy Street Audit of the pedestrian/cycle routes between the site and public transport modes should be undertaken (secured by condition / obligation. And the transport contribution within the S106 should be used to address any deficiencies identified in the audit
- 6.88 The Controlled Parking Zone contribution (secured in the outline scheme) will be used to ensure any parking demand associated with the development is mitigated controlled. Future residents of the development will be exempt from obtaining permits for any Controlled Parking Zone introduced in the vicinity of the site.
- 6.89 A site-wide Residential Travel Plan and a site-wide Non-Residential Travel Plan should be provided for Plot 08 prior to occupation, it should include a phase-specific Residential Travel Plan and phase-specific Non-Residential Travel Plan. The Plan should include details of the Car club strategy for the plot.

Housing Strategy

- 6.90 No response

Parks

- 6.91 No response

Design and Access Panel

- 6.92 The S106 agreement requires that an independent Design and Access Panel (DAP) be formed, responsible for providing advice and guidance on matters relating to design and design quality and access in relation to the development.
- 6.93 The S106 requires that the membership of the DAP comprise the following 3 persons nominated by the Council and 3 persons nominated by the owner.

6.94 The DAP met twice in relation to the proposed Reserved Matters Application for Plot 08 in March and May 2017. Following the initial meeting, the applicant responded to comments received from the panel. The outcome of the second meeting is summarised as below.

6.95 The panel supported the following:

- The overall direction the detailed design is taking
- The strengthening of the façade treatment
- The decision to focus on brick as the key material
- Greater emphasis on ground-floor character
- The introduction of colonnades
- Development of thinking about colour
- More thinking about privacy and use of frits/screens
- Development of a strong balcony strategy
- The 'open' glazed corners idea
- General approach to landscape materials
- Generous road widths and tree provision
- An element of on-street parking

6.96 The following additional points of consideration were raised:

- Consider the relationship between brick and metal, where they meet.
- Has the greater emphasis on base, middle and top resulted in a loss of the strongly vertical nature of the earlier design?
- Consider the treatment of the upper levels
- It would be helpful to develop a strategy for use of open corners not just in relation to each building but in thinking about the overall site itself
- The way the courtyard (podium) is used should be the subject of a narrative that takes account of the fact that it will be for use by residents only
- The street experience of walkers-by should be understood in order to make final decisions on landscape treatment/security
- Thinking about narrative of use could also help decisions about activities on roof terraces
- On the same basis, try to predict the likely of movement of pedestrians or cyclist based on their desire to use direct routes and/or lines of sight; this would encourage designing for desire lines, not the plan view
- The colonnade design is a great architectural opportunity; the feeling was that it is helpful to express how the blocks meet the ground rather than disguise it
- It appeared that the frit strategy for privacy could be refined to minimise cost while maximising distinctiveness.
- The panel was strongly in favour of using 'normal' London/Lewisham street signage, tree species and street furniture, making the development feel natural, rather than a developer drop-in.

6.97 The applicant has advised as to how the design has evolved and how they have responded to the comments received by the Design and Access Panel in the Design and Access Statement submitted with this application.

7.0 POLICY CONTEXT

Introduction

7.1 An application for approval of reserved matters or for discharge of/approval under conditions is not an application for planning permission. Accordingly, the provisions of Section 70(2) of the Town and Country Planning Act 1990 (as amended), which sets out

the considerations the local planning authority must have regard to in determining applications for planning permission, do not apply in the determination of this application for approval of reserved matters.

7.2 Notwithstanding the statutory provisions, there development plan for Lewisham and other policies which are relevant in assessing the current application. These are set out below.

DEVELOPMENT PLAN

7.3 The Development Plan comprises:

- London Plan Consolidated With Alterations Since 2011 (March 2016) (**LPP**)
- Lewisham Core Strategy (June 2011) (**CSP**)
- Lewisham Development Management Local Plan (November 2014) (**DMP**)
- Lewisham Site Allocations Local Plan (June 2013) (**SALP**)

7.4 The London Plan has been reviewed and a new draft London Plan produced (DLPP). This has been subject to public examination and an 'Intend to Publish' version subsequently issued by the Mayor of London in December 2019. This has now been reviewed by the Secretary of State and a response outlining amendments has been issued. The DLPP is now with the Mayor of London to informally agree amended text with the MHCLG and Secretary of State. Although not yet part of the adopted development plan, given its advanced stage the draft New London Plan carries some weight as a material consideration in planning decisions. The relevant draft policies are discussed within the report.

NATIONAL POLICY & GUIDANCE

7.5 National policy and guidance comprises the following:

- National Planning Policy Framework 2019 (**NPPF**)
- National Planning Policy Guidance
- National Design Guidance 2019

7.6 London Plan SPG/SPD:

- Planning for Equality and Diversity in London (October 2007)
- London View Management Framework (March 2012)
- All London Green Grid (March 2012)
- Play and Informal Recreation (September 2012)
- Sustainable Design and Construction (April 2014)
- Character and Context (June 2014)
- The control of dust and emissions during construction and demolition (July 2014)
- Accessible London: Achieving an Inclusive Environment (October 2014)
- Social Infrastructure (May 2015)
- Housing (March 2016)
- Homes for Londoners: Affordable Housing & Viability (August 2017)
- Culture & Night Time Economy (November 2017)
- Energy Assessment Guidance (October 2018)

NEIGHBOURHOOD PLAN

- 7.7 The application site is located within the Deptford Neighbourhood Action (DNA) (who have been recognised by Lewisham Council as a Neighbourhood Forum since February 2016) designated Neighbourhood Area. DNA are currently progressing their neighbourhood plan and Regulation 14 consultation was commenced in October 2019 – this is still ongoing. Given the early stage of the preparation of the neighbourhood plan, this does not currently carry weight in the consideration of applications.

8.0 PLANNING CONSIDERATIONS

- 8.1 The principle of comprehensive redevelopment of the site has been approved through the OPP. This permission approved the overall quantum of development and land use mix, the scale, height and massing of buildings, and the site layout and access as well as the detail of the new road layout.
- 8.2 Accordingly, the issues for consideration in the determination of the current application relate only to the Reserved Matters for Plot 08 and those details required by the conditions in respect of which discharge/approval is sought.
- 8.3 The main issues to be considered in the determination of this application and related scheme details are:
- Compliance with the Approved Development Parameters
 - Reserved Matters
 - Layout
 - Scale
 - Appearance
 - Access
 - Landscaping
 - Other details under Condition 20, Condition 21 and other Conditions
 - Environmental Considerations
 - Other Matters and Response to Objections

COMPLIANCE WITH THE APPROVED DEVELOPMENT PARAMETERS

Background to Approved Parameters

- 8.4 Condition 2 on the OPP approved and requires compliance with a series of parameter plans. Document CW05A Development Specification (dated February 2014) was also approved and required to be complied with.
- 8.5 This document provides:
- a coherent framework for the regeneration of the area;
 - a clear statement of the parameters, constraints and restrictions to which the site must adhere under the terms of the OPP; and
 - a flexible framework which is capable of responding to the needs of the scheme within the boundaries of the Environmental Statement (ES).
- 8.6 There are 18 approved Parameter Plans, which set out the parameters within which applications for approval of Reserved Matters and other approvals under the planning permission must adhere to.

8.7 The Parameter Plans are as follows:

- Parameter Plan 01 Planning Application Boundary
- Parameter Plan 02 Existing Site Levels
- Parameter Plan 03 Existing Site Sections 01
- Parameter Plan 04 Existing Site Section 02
- Parameter Plan 05 Existing Building Heights
- Parameter Plan 06 Key Development Plot Plan
- Parameter Plan 07 Proposed Site Levels
- Parameter Plan 08 Open Space
- Parameter Plan 09 Maximum Development Basement Levels
- Parameter Plan 10 Maximum Development Plot Parameters
- Parameter Plan 11 Minimum Development Plot Parameters
- Parameter Plan 12 Max/Min Development Plot Horizontal Deviation
- Parameter Plan 13 Circulation - Main Vehicular Access
- Parameter Plan 14 Circulation - Pedestrian Access
- Parameter Plan 15 Circulation - Public Transport
- Parameter Plan 16 Circulation - Cycle Routes and Cycle Stations
- Parameter Plan 17 Circulation - Parking Areas on Street
- Parameter Plan 18 Phasing Plan

Compliance with Development Plot Maximum Floorspace

8.8 The key components of Development Plot P08 are, as approved by the OPP are as follows:

- 37,400 sqm of residential (Class C3) floor space
- 1,450 sqm of shops (Class A1) and financial and professional services (Class A2) floor space; and
- 200 sqm of restaurant and cafes (Class A3) and drinking establishments (Class A4) floor space;

8.9 This Reserved Matters Application proposes 456 residential units totalling 35,750sqm (GEA) which is within the 37,400sqm maximum parameter.

8.10 At ground level 1,450sqm (GEA) of A1/A2 use and 200sqm (GEA) of A3/A4 use is proposed, which are both within the maximum approved parameters.

8.11 Thus the quantum of development proposed for Plot 08 accords with the OPP.

Compliance with Residential Mix Parameters

Housing Mix

8.12 The Development Specification also prescribes the Housing Mix under the OPP. As approved by the OPP, the development will include up to 3,500 residential units. A mix of units is proposed and 15% by units will be affordable. The affordable housing will include affordable rent and intermediate tenures as defined by the Section 106 agreement (March 2015). These figures relate to the whole of the development

Housing Type	Private	Affordable Rent	Intermediate	Total
1B/2P	40-45%	18-23%	25-30%	35-45%
2B/4P	40-45%	38-43%	65-74%	42-48%

3B6P	10-14%	27-33%	1-6%	10-14%
4B 6/7/8P	2-4%	2-7%	0%	2-4%

Table 5: Approved tenure and housing mix parameters

8.13 The proposed mix for P08 is as follows:

Housing Type	Private	Affordable Rent	Intermediate	Total
1B/2P	60%	0%	0%	60%
2B/4P	40%	0%	0%	40%
3B6P	0%	0%	0%	0%
4B 6/7/8P	0%	0%	0%	0%

Table 6: Proposed tenure and housing mix

8.14 The Council has acknowledged that there will need to be flexibility to allow the mix of individual plots and phases to respond to the characteristics and constraints of the various character areas in the Convoys Wharf Masterplan and, therefore, individual plots and phases may over or under provide against the overall targets. What is important is that scheme wide, the development is in accordance with the tenure and housing mix parameters overall. The Reserved Matters Application for P08 is the first to be submitted under the OPP. Whilst Plot 08 does not reflect the requirements of the housing mix, such mix is not required to be delivered on a plot by plot basis, but is site-wide. A Reconciliation Statement is required to be submitted with each Reserved Matters Application to demonstrate that this will be achieved on completion of the development. [A Reconciliation Statement has been submitted with the application which shows is consistent with the overall proposals for the site, as established by the Development Specification CW05A (February 2014) and Parameter Plans. The cumulative totals will inform the future development briefs for future plots to ensure that the housing mix complies with the overall requirements. Compliance with the site-wide OPP requirements will be monitored through the Reconciliation Statements.

8.15 456 net new homes are proposed for Plot 08. The current London Plan sets an annual target for the Borough of 1,385 new homes until 2025. The emerging draft London Plan, if unchanged, would increase this to 1,667. The proposal thus attributes to 33% of the annual output for the adopted London Plan target or 27% of the annual output for the Draft London Plan.

8.16 It is considered that the proposed housing mix for Plot 08 is acceptable.

Tenure Mix

8.17 The minimum provision for affordable housing under the existing Section 106 agreement is a minimum of 15% of the total dwellings (by habitable room) across the development. The tenure split is 30% Affordable Rent Dwellings and 70% Intermediate Dwellings. The overall level of provision is subject to viability review. The Section 106 Agreement also requires that not less than 15% (by Habitable Room) of the total Dwellings in Phase 1 are provided as Affordable Housing Dwellings. The Agreement does not require that all Plots must include an element of affordable housing. Rather, delivery is dealt with on a Phase by Phase basis. Plot 15 has been brought forward to enable early delivery of affordable housing. Not less than 50% of the Market Dwellings in Phase 1 are to occupied until at least 50% of the Affordable Housing Dwellings to be provided in that Phase (15% of the total dwellings by habitable room) have been completed and Transferred to a Registered

Provider and written notice of such transfer has been given to the Council. All of the Affordable Housing Dwellings are to be provided and transferred and notice given to the Council before 90% of the Market Dwellings are occupied.

- 8.18 All of the dwellings within Plot 08 are proposed as Market Dwellings. The majority of Affordable Housing Dwellings in relation to Phase 1 are proposed to be provided within Plot 15 and other development plots in this Phase (see report for Plot 15 which is also on this agenda). Whilst no affordable housing is proposed within P08, this would be provided elsewhere within Phase 1 (largely within Plot 15); and as such the proposal is in accordance with the Section 106 Agreement. Delivery will continue to be monitored through the Reconciliation Statements to be provided under Condition 8 of the OPP and the requirements of the Section 106 Agreement to ensure satisfactory provision.

Compliance with Parameter Plans

- 8.19 As stated above, the Development Specification approved 18 parameter plans. Compliance with the approved parameter plans, where relevant, is outlined in Table 7 below.

Plan No.	Title	Compliance
01	Planning Application Boundary	Y
02	Existing Site Levels	Y
03	Existing Site Sections 01	Y
04	Existing Site Section 02	Y
05	Existing Building Heights	Y
06	Key Development Plot Plan	Y
07	Proposed Site Levels	Y
08	Open Space	Y
09	Maximum Development Basement Levels	Y
10	Maximum Development Plot Parameters	Y
11	Minimum Development Plot Parameters (amended by DC/19/113231)	Y
12	Max/Min Development Plot Horizontal Deviation	Y
13	Circulation - Main Vehicular Access	Y
14	Circulation - Pedestrian Access	Y
15	Circulation - Public Transport	Y
16	Circulation - Cycle Routes and Cycle Stations	Y
17	Circulation - Parking Areas on Street	Y
18	Phasing Plan (amended by DC/18/107740)	Y

Table 7: Compliance with Parameter Plans

- 8.20 It is noted that several objections have been raised in respect of the proposed balconies overhanging the maximum development parameters.
- 8.21 General Note 03 of the Maximum Development Plot Parameters 10 (CW05A Development Specification, February 2014) states that Plot extents are subject to an additional allowance of up to 2m for balconies, bays, winter gardens, canopies, and awnings projecting out from plot extent limited to within Planning Application Boundary.
- 8.22 The proposed balconies for P08 would project a maximum of 1.7m beyond the proposed building envelope, and as such are within the approved development parameters.

RESERVED MATTERS

Layout

Policy

- 8.23 Core Strategy Policy 15 (High quality design for Lewisham) sets out the general objectives and approach to securing design quality in new development across the borough and Policy 18 provides more detailed guidance on the design (as well as location) of tall buildings. In respect of Convoys Wharf itself, Strategic Site Allocation 2 sets out a number of urban design principles for the development of the site.
- 8.24 The NPPF also highlights the importance of high quality and inclusive design, and of achieving a good standard of amenity for all existing and future occupants of land and buildings. The NPPF also notes that good design is a key aspect of sustainable development, which includes delivering a wide choice of high quality homes.
- 8.25 LPP 7.1(d) states the design of new buildings and the spaces they create should help reinforce or enhance the character, legibility, permeability, and accessibility of the neighbourhood.

Discussion

- 8.26 The layout of the Plot, including the siting of the proposed building, is outlined in Image 3 below:

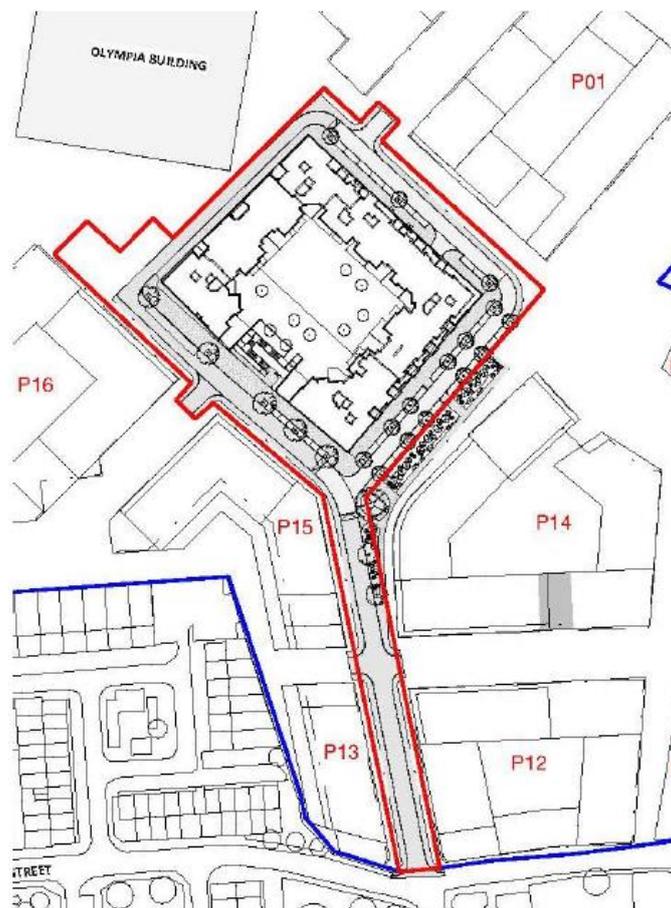


Image 3: Proposed siting and layout of Plot 08 in relation to surrounding plots

- 8.27 The layout is largely informed and constrained by the parameter plans and layout approved by the OPP. Whilst the OPP establishes the general layout, it does allow for flexibility in the detailed design and layout of the individual plots to create variety and architectural subtlety. The layout of the Development has been developed within the parameters of the OPP (and approved non-material amendment) and has also established the following key principles in relation to the detailed layout. The layout should:

- Adopt the principles of ‘Secure by Design’, creating permeable divisions between public and private areas
- Allow for good natural surveillance of all public spaces
- Provide strong active frontages
- Provide a maximum number of dual aspect units
- Provide privacy and positive outlook for all units
- Provide efficient servicing arrangements

- 8.28 Plot P08 falls in two of the defined character areas under the Site Wide Heritage Statement submitted under Condition 13 - the Eastern Gateway and Olympia Square, furthermore plot P08 faces three of the Landscaped Public Spaces; Olympia Way South, Olympia Square and Royal Caroline Square. The design response to these adjacencies is discussed in more detail below.
- 8.29 The layout of the access, routes and connections to surrounds are adhered to with consideration to positioning of retail usage and access points into Plot 08 to address the layout of these streets.
- 8.30 The building on the Plot comprises a ‘U’ shaped building upon a double height commercial podium at ground level with parking located in the centre. A shoulder height of 10 storeys would encircle the podium with a gap or break located at the south western to permit access to landscaping which would be located atop the podium. Two 14 storey feature buildings would be located on the south eastern corner, at the apex connecting to New King Street and the north western corner facing Olympia Square. A further 13 storey building would be located to the south western corner, also facing Olympia Square. The layout is fixed in this manner to have the opening to the podium courtyard facing south, and thus receive as much sunlight and daylight as possible. In the context of the approved parameters, the layout proposed is considered optimal, maximising sunlight and daylight to the proposed units and the standard of accommodation to be provided. The description of a development of 15 storeys refers to the inclusion of a two storey-parking podium concealed within the block. Externally facing the street, there would be 14 storeys of accommodation.
- 8.31 The new route to the river from Deptford High street via New King Street splits at the building on Plot 08 and offers pedestrians two routes to two of the visible heritage assets on the site, The Dry Dock and the Olympia Building. This is the layout as defined and approved by the OPP parameters which was developed so that Plot 08 forms the fourth side to Olympia Square. The new surrounding buildings that form Olympia square are set to a different geometry and scale to the Olympia building deliberately allowing the Olympia building to read as independent and unique as one of the few heritage assets intact within the site area. This arrangement allows the Olympia building to be at the centre of the scheme within a pedestrian public square of activity surrounding the structure.
- 8.32 The relationship of each building to the other has been carefully designed from the street level experience so the public realm repeatedly expands and contracts as a pedestrian moves towards the river. The points of contraction are organised at the lowest part of the unique Olympia roof form. Additionally, the proposed colonnade has been designed to provide a ‘soft’ edge to the square and this point of contraction.
- 8.33 Further comments on public realm are included below in the section on the influence of heritage on the design of Plot 08.
- 8.34 The proposed layout of Plot 08 is in accordance with the principles and parameters of the OPP and is considered acceptable.

Scale

Policy

- 8.35 Planning should promote local character. The successful integration of all forms of new development with their surrounding context is an important design objective (NPPG).
- 8.36 LPP 7.4 expects development to have regard to the form, function and structure of an area, place or street and the scale, mass and orientation of surrounding buildings. LPP 7.6 states architecture should make a positive contribution to a coherent public realm, streetscape and wider cityscape.

Discussion

- 8.37 The OPP Parameter Plans establish a variety of scales of buildings on the wider Convoys Wharf development, including low, medium and high-rise buildings that respond to the existing and emerging context of the area, including the proposed character areas.
- 8.38 The locations and maximum heights of the buildings are established in the OPP and cannot be reconsidered in the determination of the Reserved Matters Applications. The proposed development is located within the parameters as defined by the OPP.
- 8.39 A clear massing strategy has been developed that builds on the OPP, which indicates taller corner elements with mid-rise shoulders in between, and an opening on the south-west façade to provide daylight into the podium. The mid-rise elements of the Development relate to the existing surrounding housing, with the higher elements relating to the larger scale of development proposed with the Convoys Wharf development, with setbacks included to reduce the appeared scale of some of the buildings allowing articulation in the facades to break up the massing. This strategy has been tested through a series of models, technical environmental testing (such as daylight, sunlight, overshadowing and wind analysis), and views testing.
- 8.40 The approach to the scale and design of Plot 08 is indicated in Image 4 below:

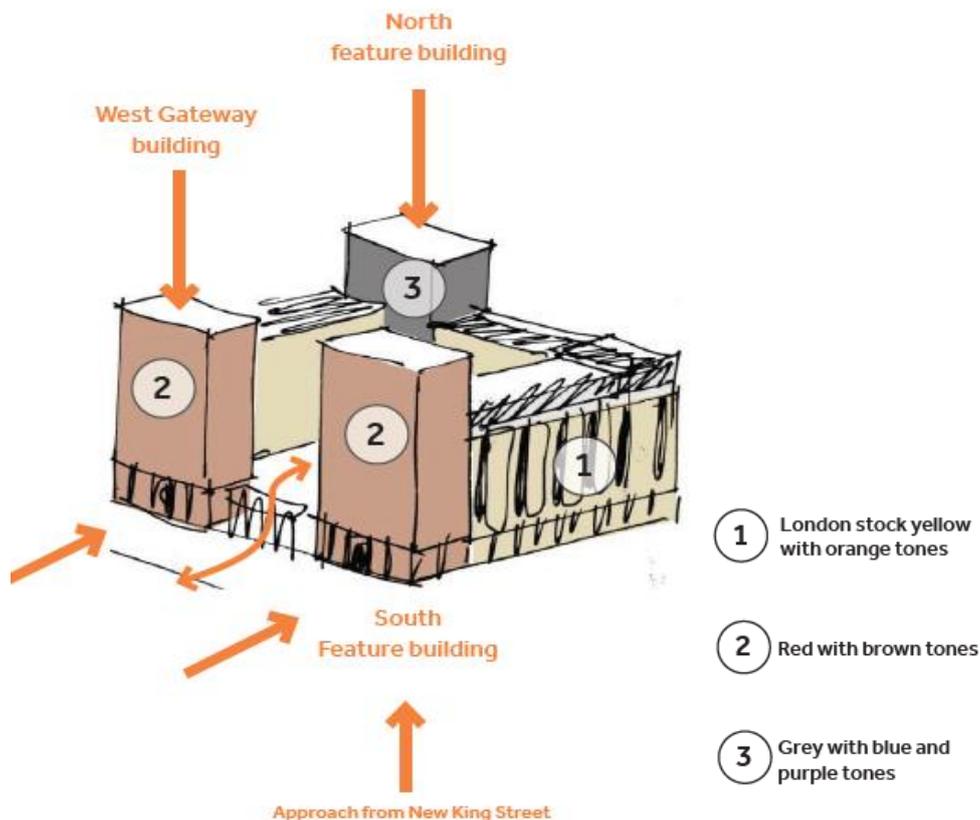


Image 4: Proposed siting and layout of Plot 08

- 8.41 The tallest elements on the north east and southwest corners of the plot, are located at points of significance, reinforcing the legibility of Convoys Wharf through its location linking Deptford High Street to the River Thames. The building is in context with a number of surrounding buildings, as well as the scale of the surrounding proposed plots, which will include tall buildings. The step down from Plot 08 will continue with outer plots towards the scale of the existing urban fabric to the south of the plots.
- 8.42 The massing and scale principles have been adhered to with consideration of choice of and articulation of materials to help define the different parts of the structure. In particular key use of materials such as three different brick colours and metal panels to distinguish between mid-rise and feature buildings, helping to break down the built mass into different elements.
- 8.43 Materials are discussed in further detail below in relation to 'Appearance' and design.
- 8.44 A Sunlight and Daylight Report has also been submitted as required by Condition 4 of the OPP to inform the design of building height and massing. Details of this are set out at "Sunlight and Daylight to Proposed Units – Condition 4" below.
- 8.45 The proposed scale and massing of Plot 08 is within the OPP Parameters and are considered to promote a high quality of design, as such, the proposals for Plot 08 are considered acceptable in this regard.

Appearance

Policy

- 8.46 In terms of architectural style, the NPPF encourages development to be sympathetic to local character and history, including the surrounding built environment and landscape setting, while not preventing or discouraging appropriate innovation or change (para 127). At para 131, the NPPF states great weight should be given to outstanding or innovative designs which promote high levels of sustainability, or help raise the standard of design more generally in an area, so long as they fit with the overall form and layout of their surroundings.

Discussion

- 8.48 The proposed materials of the building are as follows:

Area	Proposed Material
Main external walls	1. Yellow buff brick 2. Red brick 3. Grey brick
Roof	Green and brown roofs
Shopfront surrounds	Precast stone
Windows	Anthracite powder coated aluminium
Balconies	Glass cantilevered
Metal panelling	Champagne metallic

Table 8: Proposed materials

- 8.49 The principle employed in developing the appearance of the proposed development has been to use a palette of high quality materials coupled with simple, crisp detailing. It is also important that the materials age well and are low maintenance in order to ensure that the area will continue to look better over time.
- 8.50 It is proposed that the architecture for the development will provide a sense of rhythm along the length of the frontages. Further variation within each street will be provided through the location of retail units on a number of frontages, mix of house types, window proportions,

relief and façade colour. These factors will help create a sense of variety and interest across the streetscapes that will respond to the differing character areas defined in the Outline Planning Permission. The architects have undertaken an analysis of the local context, building styles and typologies to ensure the proposed development ties in with the local vernacular.

- 8.51 The use of brickwork throughout Plot 08 is a response to the local character. However, the inclusion of metal cladding and glazing, are proposed to create a sense of architectural diversity and character across the site.
- 8.52 The distribution of the brick colour pallet has been used to identify the overall massing of the Plot, to give a clear identity to the three main corner towers and to unite the remaining lower level building mass. To the north-east and south-east, where the shoulder of the building is set back, a change in material from brick to metal cladding helps to break down the massing of the overall built form.
- 8.53 Image 5 below taken from the Design and Access Statement gives a computerised image of how the proposed building would appear in its context.



Image 5: View from west, north of the Olympia building (outline of Olympia to left of image)

- 8.54 The massing and scale principles have been adhered to with consideration of choice of and articulation of materials to help define the different parts of the structure. In particular key use of materials such as three different brick colours and metal panels to distinguish between mid-rise and feature buildings, helping to break down the built mass into different elements. The applicant has stated that the use of metal spandrel panels and recessed facades reflects the heavy use of metal within the Olympia Building to both the interior and exterior of the structure.
- 8.55 The colours of the brick relate to existing local brick colours and tones the architects have identified in their character assessment of the wider built environment. The positioning the brick colours around the site differentiate the building mass and give orientation to the building, but more importantly to create feature buildings expressing the 'node' corners.

- 8.56 The two red brick feature buildings to the south side, frame the opening or gateway into the podium space with the right hand (south-east) red brick part of the building acting as the key node to this corner of the site. The grey brick feature building defines the northwest corner of the site, marking the east corner of Olympia square and the approach to the waterfront. A buff London yellow brick is used as the main uniting material choice for the remaining primary façades. The grey and red bricks never directly meet, and all brick colours are broken by a recessed metal panel.
- 8.57 At ground floor level, the proposed retail units would have a double height glass frontage with designated signage zone for uniformity. The retail frontage to Olympia Square and Eastgate sit back behind the colonnade. The retail frontage to Olympia Way North and South sit on the main building line, but take on the same language of the primary frontage with the shopfront glazing recessed behind stone cladding and brick detailed columns coming to ground.
- 8.58 Further comments on architectural treatment and materials are set out below in the section dealing with the influence of heritage assets in the design of Plot 08.
- 8.59 The strategy to the external appearance of Plot 08, coupled with the overall design of the building is considered to be an appropriate response to the plot's location in the site and to respect the heritage assets and historic significance of Convoys Wharf as well as that of the surrounding area. The architectural treatment of the building and landscape will set a benchmark for future development of the rest of the plots.

Access

Policy

- 8.60 The NPPF requires safe and suitable access for all users. Paragraph 108 states that in assessing applications for development it should be ensured that appropriate opportunities to promote sustainable transport modes can – or have been taken up and that amongst other things safe and suitable access to the site can be achieved for all users.
- 8.61 CSP 14, states amongst other things, that the access and safety of pedestrians and cyclists will be promoted and prioritised; that a restrained approach to parking provision will be adopted; and that car-free status for new development can only be assured where on-street parking is managed so as to prevent parking demand being displaced from the development onto the street.
- 8.62 A 'Healthy Streets' report has also been submitted in support of the applicant and this is discussed further at the "Healthy Streets" section of this report below.

Discussion

- 8.63 As Plot 08 is proposed as the first plot to be developed there will be a degree of temporary works with regard to access and public realm. This is indicated by Image 6 below. All works will be undertaken initially with the works indicated on the right of image 6 to be only temporary and permanent works to be delivered as other plots come forward.



Image 6: Permanent works (left) and temporary works (right) for access

Pedestrian and Cycle Access

- 8.64 Improving pedestrian access and permeability within, to and from the site is a key objective of the OPP. The approved Design Guidelines sets out key design commitments and framework principles relating to movement. It was developed in response to local pedestrian movement patterns and an aspiration to reconnect the district with a series of routes that integrate with the wider context and break down the barriers of the former Convoys Wharf site.
- 8.65 With regard to Plot 08 specifically, pedestrian and cycle access will be from New King Street. New pedestrian and cycle routes will be provided throughout the Site in accordance with the OPP, created along the Spine Road and from New King Street towards the River Thames.
- 8.66 Given P08 is the first Reserved Matters Application to come forward, there would be no pedestrian or cycle access through the site provided initially. Rather there would be one point of access from New King Street directly to P08 with a road and footpaths encircling the plot (see Image 3). These roads and accesses are as per those approved at outline planning stage and would eventually be connected to a wider network of roads and footpaths as other plots of the development come forward.
- 8.67 Cycle access would be provided via a shared vehicular and cycle two-way 5.5m to 6m carriageway. The details provided by the applicant indicate that there would be sufficient access for bicycles. Full details of the proposed cycleways and how these connect to the existing cycle network have not been provided but are required as a pre-commencement detail by condition 32 of the OPP.
- 8.68 Pedestrian access specifically would be provided by footways on either side of the access road. These will typically be circa 5-8m but will increase to 11m in certain areas and decrease to 3m at certain pinch points. All the pedestrian access footways would be public. Officers have reviewed the footpath widths and are satisfied that whilst constrained by the

OPP parameters, that these would be sufficient to permit movement around the development site whilst respecting current social distancing guidelines.

- 8.69 It is noted that the permanent layout of access along the whole length of the spine road will be approved as further applications come forward in relation to the Plots.
- 8.70 For all residential homes, pedestrian access to the common cores is proposed directly from the street through 5 separate cores positioned at each of the corners with an additional core on the north eastern elevation, as well as from the communal residential courtyards at podium level. For the proposed retail units, level access would also be provided from street level.
- 8.71 The residential cycle parking spaces would be provided at first floor level within the car park. Access to the residential and cycle parking spaces would be from ground level and first floor access would be gained via lifts positioned at each core. The proposed retail unit cycle stores would be located at and accessed from ground floor level. The details of the actual cycle parking provision are reserved by condition 33.
- 8.72 The proposed pedestrian and cycle access complies with the parameter plans and is considered to be safe and convenient and in accordance with the aims and objectives of the OPP.

Vehicular Access and Access to Parking

- 8.73 The vehicular access would also be gained via the two-way 5.5m to 6m carriageway which would run from New King Street and which would encircle the development plot. This would also be connected to a wider vehicular network as future plots are developed.
- 8.74 The access to residents parking would be located at ground level on the north eastern elevation of the proposed block. This is in accordance with the access arrangements as defined by the approved Development Specification. Parking provision itself would be provided at first floor level with access to each of the five cores provided directly from the car park.
- 8.75 On-street parking provision would be located along 'Royal Caroline Square' together with on-street electric vehicle parking bays.
- 8.76 The full details of parking provision (including disabled parking), electric vehicle charging point and car park management are all reserved by condition (Conditions 25, 31 and 50).

Surface Treatments

- 8.77 A mixture of permanent and temporary surface treatments would be provided as per Image 9 above. The temporary surface treatments would consist of vehicular and pedestrian grade asphalt, which would be replaced as future adjacent development plots are developed.
- 8.78 The proposed permanent surface treatments are outlined in Table 9 below.

Area	Proposed Material
On-street parking bays	Marshalls Myriad Block Paving
Pedestrian footways	Marshalls Conservation Flag Paving (granite)
Tactile paving	Marshalls tactile paving
Vehicular and cycle carriageway	1. Vehicle grade asphalt to spine road 2. Marshalls Myriad paving to 'Royal Caroline Square' and 'Olympia Way North'

Table 9: Proposed permanent surface treatment

8.79 The proposed permanent surface treatments have been reviewed by officers and are considered to be of a high quality and sufficiently durable and fit for purpose. For continuity and achieving a high standard overall design quality, it is expected that these high quality materials will be carried through to other plots as future Reserved Matters Applications and other details come forward.

Landscaping

Policy

8.80 LPP 7.5 relates to public realm and expects public spaces to among other things be secure, accessible, inclusive, connected, incorporate the highest quality design and landscaping.

8.81 DM Policy 35 states that Public spaces should be designed to be safe, inclusive, accessible, attractive and robust, enhancing existing connections and providing new connections as appropriate. Existing local and historic connections that are valued and contribute to the distinctiveness of the area's public realm and streetscape should be enhanced. The provision of public art will be encouraged.

8.82 The proposals include a large area of landscaping defined within the development specification approved under the OPP as "Private Open Space on Podium Level". It is shown on Image 7 below and has thus been determined by the OPP and the principle of this as private space accepted.



Image 7: Areas of public and private open space at and surrounding P08 (from OPP)

Discussion

8.83 The applicant has stated that the design option focuses on the straight lines and linear arrangements, drawing on the primary geometry of John Evelyn's design for Sayes Court Gardens. Large areas of open lawn create open spaces for people to sit, while planted areas create spaces for quiet conversation and reflection. The podium programme for the Plot 08 podium landscaping is outlined in Image 8 below.

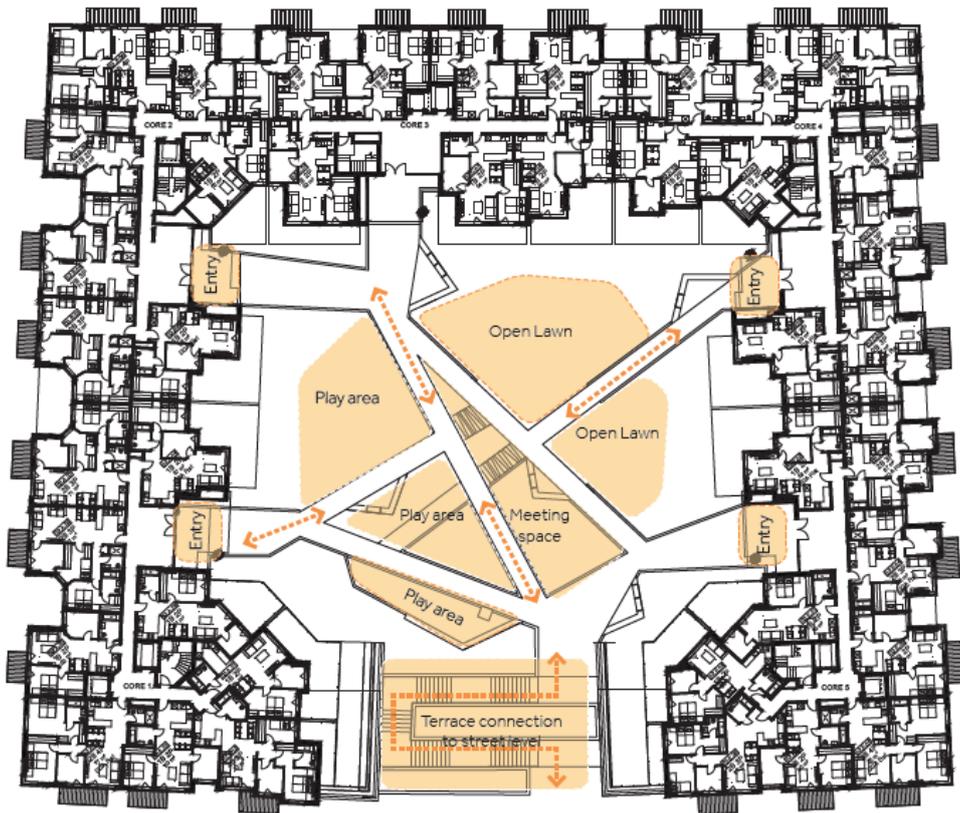


Image 8: Podium programme for P08 landscaping

- 8.84 The design of the podium incorporates level changes in order to accommodate trees on both sides of the space, while also creating an interesting environment for people and seating edges to planters. Play is accommodated by means of a playable landscape area, which wraps around the central space, and is integrated into the overall design while also offering play value for younger children. This is discussed further below.
- 8.85 The materials used will be concrete paving blocks for main entrances, building entrances and sitting areas with resin bound gravel used for the internal paths. Timber seats will also be incorporated into the design which would include LED strip lighting for wayfinding. Other lighting would include uplighters to feature trees, lighting to bollards to mark play paths and lighting incorporated into handrails.
- 8.86 With regard to soft landscaping, drought resistant lawn and bulbs with four 5-8m high trees and a series of five more smaller flowering/fruited trees are proposed to the south of the podium area. Grass planting mixes would also be incorporated along with accent plants and climbers to dedicated areas of soft landscaping.
- 8.87 Timber fencing (1.8m to 1.5m in height) would be used along the edge of the podium area where the amenity space meets private terraces to afford appropriate privacy to the occupants of the residential units adjacent to the podium courtyard.
- 8.88 Steps would be located to the south western elevation of the block allowing direct access from public footways to the podium space, for occupants of P08. Level access could be gained via lifts at ground floor level in any of the five proposed cores. For security, a steel fence would be provided at ground floor level along the south western boundary, this would consist of upright steel bars (to match the metal panelling elsewhere on P08) with no vertical bars to give an open appearance whilst maintaining an appropriate degree of security.
- 8.89 Further comments on landscaping matters are included below in the section on the influence of heritage on the design of Plot 08.

8.90 Overall, the design, materials and species selection of the landscaping is considered to be of a high quality and thus acceptable.

Playspace Provision

8.91 With regard to playspace provision, the strategy for the provision of such is described in the OPP. The following table from the applicant’s Outline Planning submission indicates the approach to provision of playspace.

Age Group	No of Children	Area Required	On-Site Provision	On site Area Provided	Off-Site Provision	Off-Site Area	Total	Over Capacity	
<5	256 (57%)	2583 m ²	Semi-private Courtyards 14,540m ²	16,766 m ²	Neighbourhood Playable Space/ NEAPS 1,800 m ²	Neighbourhood Playable Space/ NEAPS 29372 m ²	0 m ²	16,766 m ²	+14,183 m ²
			Doorstep Play 1,200 m ²						
5 to 11	116 (26%)	1158 m ²	Local Playable Space / LEAPS 300m ²	768 m ²	Local Playable Space / LEAPS 450m ²	8,075 m ²	8,843 m ²	+7,685m ²	
12+	73 (17%)	713 m ²	Youth Space 0m ²	0m ²	Youth Space 665m ²	5,558 m ²	5,558 m ²	+4,845m ²	
TOTAL	445	4453 m²		17,534 m²		13,633 m²	31,167 m²	+26,714m²	

Image 9: Outline Planning Permission playspace calculations

8.92 The approach was that playspace for below 5 year olds and 5 to 11 year olds would be provided for (in excess) on site with provision for 12 plus year olds provided on existing facilities off-site.

8.93 The proposals for Plot 08 play areas for children under 5 would be in accordance with GLA guidance on play provision. The total required playable area for Plot 08 is 442.8 m2 (standard of 10 sqm per child)(blended figure for PTAL 0-3) for children of all ages from 0-17 years old. The scheme as proposed would provide 278sqm dedicated playspace in total, with other incidental playspace located elsewhere in the communal amenity space to the rear of the proposed building. In addition to this space, a further 246sqm of open lawn space is also provided at podium level. This amounts to a total provision of playspace and open lawn at podium level to 524sqm.

8.94 As outlined above, the proposed OPP envisaged playspace for 5-11 and 12+ year olds being provided off-site. Of the 442.8sqm required for P08, 249sqm is required for 0-5 year olds – as such, the proposals for P08 meet the requirement for children aged 0-5 (provision of 278sqm). Furthermore, when including the open lawn space proposed, the proposals for P08 would provide in excess of the total playspace for 0-17 year olds providing a total of 524sqm against a requirement of 442.8sqm. This in turn would reduce reliance on playspace not located on the application site.

8.95 Play provision at podium level will be integrated into the overall design for the communal amenity space, and consist of a combination of medium sized and smaller play spaces, joined by informal paths through the landscape. These areas will be designed with reference to accessible play guidance such as Developing Accessible Play Space: A Good Practice Guide. The dedicated play space is indicated above in Image 12.

8.96 It is also noted that in addition to the additional playspace provided on the application site, a Local Open Space Contribution of £560,000 has been secured to be used specifically for “improvements to all or any of the existing public park known as Sayes Court Gardens and other open spaces and play areas within the vicinity of the Development.”

8.97 The proposed approach to playspace provision is considered acceptable and in accordance with the parameters and principles of the OPP. Officers are satisfied that within the parameters of the maximum quantum of space available for playspace has been delivered.

Other Matters

Heritage Assets

Background

8.98 Deptford in general and the application site in particular have a long history of maritime heritage. The site includes many areas of known archaeology and in-filled docks and basins and a Scheduled Ancient Monument. There is also the Grade II listed building – the Olympia Warehouse and the Grade II listed entrance gate and part of the perimeter wall. Adjoining the site to the southeast is the listed Shipwrights Palace. The archaeology places restrictions on the building format and thus necessitates the use of extensive podium levels approved at Outline Planning Application stage.

8.99 With this wealth of historic maritime connections, some of them relating to the Royal Family and explorers such as Drake and Raleigh, the site has been recognised as having opportunity for the creation of a distinctive place/series of places. The OPP stated that this should be brought about in a meaningful way at the detailed stages of the scheme.

8.100 The Convoys Wharf Site is not within a Conservation Area, nor in close proximity to one. The closest is the Deptford High Street and St. Paul’s Conservation Area, that sits at circa 150m and 200m from the Site respectively. There is some limited intervisibility between the south-eastern section of the Site, looking down New King Street, with the northernmost edge of the Deptford High Street Conservation Area and there will thus be some extremely limited intervisibility between the south-eastern, upper levels of Plot 08 with this conservation area.

8.101 The Greater London Historic Environment Record (GLHER), records heritage assets found within this 0.25km search radius; these are illustrated in table 10 below. Listed heritage assets within this area that may be affected by the Proposed Development are detailed in the table below:

Listed Structures	Grade
Former Master Shipwright’s House	II*
Former Office Building of Royal Dockyard	II*
Olympia Building	II
Boundary Wall to Convoys Wharf	II
Paynes Wharf	II
River Wall	II

Table 10: Designated heritage assets within 0.25km from site

8.102 The Olympia Building (Grade II Listed) is immediately adjacent to Plot 08 on its north side. The Olympia building is one of only 7 such structures to survive nationally. It was built in 1844-46 to cover slips 2 & 3, and was altered with wrought iron tied arch roofs between 1880 and 1913, with the roof profile altered from pitched to arched. It is the only above ground building on site remaining from the Dockyard period and its central position in the site underpins its importance in revealing the history of the Dockyard. Its connection with the river is at the heart of its significance, but its roof profile and internal structure when seen from several viewpoints will also be of significance in revealing the history of the site.

8.103 Further to the above, the Scheduled Monument, that is the Tudor Naval Storehouse, is located to the north of the plot within the development site; however, it has been excavated and preserved in-situ and is therefore not visible above ground.

- 8.104 Plot 08 is located in a part of the Dockyard that was largely open yardage with a small number of ancillary structures and was only comprehensively developed after the Dockyard had closed. Archaeological evaluation trenching has revealed no significant Dockyard structures here – though there is evidence for widespread ground disturbance from late nineteenth and twentieth century development.

Conservation Areas and Listed Buildings/Structures

- 8.105 Lengthy consideration was given to heritage issues at the OPP stage. When granting the OPP, the Mayor considered the development would appropriately ensure the preservation of existing archaeology at the site, the significance of the Olympia building (Grade II) and Master shipwrights House and Dockyard Officer (Grade II*) and would enhance the settings of these Listed Buildings. The proposal would not cause harm to the setting or significance of the other Listed Buildings at the site, or in the surrounding townscape and would also preserve the character of Deptford High Street, West Greenwich and Greenwich Park Conservation Areas.

Policy

- 8.106 Relevant paragraphs of Chapter 16 of the NPPF set out how LPAs should approach determining applications that affect heritage assets. LPAs are required to identify and assess the significance of a designated heritage asset. When considering the impact of proposals on designated heritage assets great weight is to be given to the asset's conservation and any harm to or loss of the significance of such assets requires clear and convincing justification. Thus, the provisions of the NPPF import a requirement to identify whether there is any harm to designated heritage assets and if so to assess the impact of such harm. If there is harm, paragraphs 195 and 196 of the NPPF are then engaged according to whether the harm is substantial or less than substantial.
- 8.107 LPP 7.8 states that development should among other things conserve and incorporate heritage assets where appropriate. Where it would affect heritage assets, development should be sympathetic to their form, scale, materials and architectural details. DLPP HC1 reflects adopted policy.
- 8.108 CSP 16 ensures the value and significance of the borough's heritage assets are among things enhanced and conserved in line with national and regional policy.
- 8.109 DMP 36 echoes national and regional policy and summarises the steps the borough will take to manage changes to Conservation Areas, Listed Buildings, Scheduled Ancient Monuments and Registered Parks and Gardens so that their value and significance as designated heritage assets is maintained and enhanced.

Discussion

- 8.110 As above, the Convoys Wharf Site is not within a Conservation Area, nor in close proximity to one. The closest is the Deptford High Street and St. Paul's Conservation Area, that sits at circa 150m and 200m from the Site respectively and there is limited intervisibility between the proposed building and these Conservation Areas. Plot 08 would be located in excess of 350m from the Conservation Area.
- 8.111 Given the distance and the limited intervisibility between the development and the nearest conservation areas, it is considered that the proposals would result in no harm to these heritage assets.
- 8.112 With the exception of the Olympia Building, in regard to the listed structures as outlined in Table 7 above, it is also considered that given the distances between such and the proposed building and the limited intervisibility between such, there would be no harm to

the setting of these assets. Furthermore, it is noted that the proposed scale and massing of the building is within the parameters as defined and approved by the OPP.

- 8.113 With regard to the impact on the setting of Olympia building, Plot 08's north west elevation will provide the enclosure of the south eastern side of the square that Olympia Building is at the centre of. This elevation at ground floor will have an arcaded ground floor with retail units along its length, potentially creating an active and vibrant street edge. It is considered that the architectural approach of strong active base and vertically emphasised upper levels is an appropriate response and will provide a strong edge to the square.
- 8.114 In relation to the Olympia building, the Council's Conservation Officer considers that its large footprint, dramatic roofscape and its position as centrepiece in the new large public square will give it a new significance within the site. Its preserved/reinstated connection with the river Thames will serve to preserve its historic integrity and reveal its former function, and proposals for refurbishment and a new viable use will be of benefit to its long term survival. The Conservation Officer has concluded that this proposal will preserve and not cause harm to the heritage asset.
- 8.115 Further to the above, Historic England (Designated Built Heritage Assets) have been consulted on the application and have stated they do not have any comments in relation to the proposals for Plot 08.
- 8.116 It considered that the proposals are acceptable with regard to with regard to Impact on Conservation Areas and Listed Buildings/Structures.

Archaeology

- 8.117 The majority of archaeological interest on site is fragile (with the exception of robust stones to the dock entrances which would be revealed where possible and the Sayes Court manor house foundations). As such, the approach taken to the management of archaeology generally, as outlined in the OPP, has been to preserve the remains in situ. The scheme was considered acceptable with regard to archaeology at OPP stage, subject to the following pre-commencement conditions. The full wording of each condition is detailed in the OPP at Appendix 1.
- Condition 34 (Scheme of Archaeological Management)
 - Condition 35 (Programme of Archaeological work)
 - Condition 36 (Programme of Archaeological Recording – Historic Buildings)
 - Condition 37 (Details of Development below Ground Level)
 - Condition 38 (Design and method statement for foundation design and ground works)
 - Condition 39 (Demarcation and safeguarding of archaeological remains)
- 8.118 The current application as originally submitted sought discharge of these conditions. Following consultation with Historic England, however, it was concluded that there was insufficient detail within the submission to permit discharge. Discharge of these conditions has now been removed from the scope of the application. It should be noted that this suite of conditions must all be discharged prior to commencement of works on the Plot.

Influence of Heritage Assets on Proposed Design

- 8.119 Condition 13 requires each Reserved Matters application to be accompanied by a Heritage Statement demonstrating how the design (including but not limited to layout, public realm, architectural treatment and materials) has been informed by heritage assets, both above and below ground. In this regard, the following documents are relevant:
- Design and Access Statement

- Design Guidelines; reference CW04
- Heritage statement (plot specific)
- Heritage Statement (site wide) (April 2013); reference CW014

8.120 The OPP was accompanied by a site wide Heritage Statement which outlined the following:

“Convoys Wharf site exhibits a high level of historic significance, but relatively few historic features survive. The overall aims have been to preserve the significance of the surviving elements of the site’s heritage, and to allow the heritage to inform the character of the new development and so to contribute to the overall success of the place. In terms of the built structures, this approach leads to stabilising, restoring and adapting the elements to a new use and providing a new setting for them.”

8.121 In reference to the unique and high levels of historic significance of the development site, and the approach outlined by the Heritage Statement above, Condition 13 was imposed. The initial submission included a Heritage Statement in respect of Condition 13. References to how heritage had influenced the proposed design were, however, limited to references to brick choices and arcades at ground floor referencing the neighbouring Paynes Wharf river frontage. Additionally, the statement did not include any reference to below ground heritage assets. The initial statement was therefore considered inadequate to satisfy the purpose of this condition as it did not adequately demonstrate how the proposed design had been informed by the site’s heritage assets. The applicant was advised to review the scheme accordingly.

8.122 Subsequently the applicant entered into a process of amending the scheme to better reflect the heritage assets and history of the site. This process involved a series of meetings with the planning department including Conservation, and Historic England (Greater London Archaeology Advisory Service).

8.123 During this process, it was agreed that the applicant should produce a Site Wide Heritage Design document. The purpose of this document is to serve as a guide for all design team professionals (subject to public consultation) involved in the scheme, advising how, why and where design should reference the heritage and history of the site.

8.124 An initial draft of this document was submitted to the Council and Historic England in December 2019 and provided an initial structure and framework as to how the site’s history and heritage could be reflected through design. The document is supported and it is envisaged that this be a ‘living document’ that continues to evolve and adapt through continued development in coordination with the local community.

8.125 The initial Site Wide Heritage Design document sought to divide the site into seven separate character areas, each reflecting a unique chapter and era in the site’s extensive history. The character areas are outlined in Image 10 below:



Image 10: Character areas as identified by the Site Wide Heritage Design document

- 8.126 Plot 08 is located within the Timberyards and Sawmill character area as identified by this document. The document states the following with regard to Plot 08:

“Plot 08 lies on what was timber/plank sheds and sawmill. Firstly, we know that the floor plan of the shed structures was two parallel rectangles with main axes north to south. An early illustration shows that the roof was a regular triangle shape.

We also have the measurements of the brick foundations that contained a central drainpipe. The wood stored was primarily oak, piles of these long timbers were stored within the sheds and also used as a foundation for some areas of the site. The findings also describe a cobbled road surface at the east of the plank shed with lines of longer cobbles seeming to form a kerb.

What can be taken from this data is primarily use. Triangular roofing and the long slim length of the two buildings are not necessarily reusable for modern building typologies but considering materials it may be possible to reference the use of timber as a way of bringing the sheds and sawmill back to life.”

- 8.127 The document goes on to provide precedents of how heritage interventions in architecture have been employed elsewhere across the UK and further afield. Using this document as a framework for design reference to heritage assets, the architects for Plot 08 (Farrells) and masterplan architects for the Convoys Wharf development, produced a document of design responses for Plot 08. This sets out in detail how the proposals have been influenced by the above and below ground heritage assets of the development site and is acceptable to discharge condition 13. It is acknowledged, however, that this document is a 'living

document' and will develop and evolve constantly as the development progresses through consultation and input from various stakeholders and the community.

Summary of Plot 08 Specific Above and Below Ground Heritage Assets

- 8.128 The architects have acknowledged the fact that the area on which Plot P08 sits does not include any of the existing principal heritage assets identified, although it was the location of timber sheds, sawpits and minor buildings for timber preparation.
- 8.129 As there is a lack of visible heritage within this plot, the building at P08 has a different role within the overall scheme, and its primary purpose has been identified as enabling the above and below ground heritage assets of the wider site to flourish. Notwithstanding such, references of the history and heritage of the site in the plot design are outlined above in the section on layout.

Relationship with the Olympia Building

- 8.130 The building on plot P08 has an important role in the site. The new route to the river from Deptford High street via New King Street splits at this building and offers pedestrians two routes to two of the visible heritage assets on the site, The Dry Dock and the Olympia Building. This is the layout as defined and approved by the OPP. There would be a change to the setting of the Olympia building as permitted in the Outline Scheme, but views toward the building from the new square and public realm would enable visitors to appreciate this structure in a way that previously has not been possible, given the industrial use and history.
- 8.131 The OPP was designed in so that Plot 08 forms the fourth side to Olympia Square. The new surrounding buildings that form Olympia square are set to a different geometry and scale to the Olympia building deliberately allowing the Olympia building to read as independent and unique as one of the few heritage assets intact within the site area. This arrangement allows the Olympia building to be at the centre of the site within a pedestrian public square of activity surrounding the structure.
- 8.132 The relationship of each building to the other has been carefully designed from the street level experience so the public realm repeatedly expands and contracts as a pedestrian moves towards the river. The points of contraction are organised at the lowest part of the unique Olympia roof form. Additionally, the proposed colonnade has been designed to provide a 'soft' edge to the square and this point of contraction.

Architectural Treatment and Materials

- 8.133 The building at P08 is designed with a contemporary interpretation of the riverside arches displayed in the existing maritime waterfront architecture of the Payne's Wharf building and the existing buildings on Foreshore.
- 8.134 The building materials have been chosen to reflect the local vernacular with the predominant use of brickwork, the colour and texture of which represents the local palette of variegated bricks found in the listed river wall, the listed perimeter wall and the local listed buildings. The use of metal panels reflects the metal panels to the east side and roof of the Olympia building.
- 8.135 The design team considered whether to reference the historic uses on the site with the use of timber on the building facades that might reflect how the area had been used during the significant period of shipbuilding. The use of timber on the facades was however discounted because it was considered ill-advised and would raise concerns with regard to fire safety.

Public Realm

- 8.136 The underlying key themes derived from the history and heritage of the site, identified in the OPP Design Guidelines as having influence on the public realm design, have been referenced in the Plot 08 landscape proposals. These are namely through the use of planting typologies and species which make subtle reference to the influence of John Evelyn.
- 8.137 The Design Guidelines identify other areas of the site (namely Evelyn Gardens) as being a key space where more explicit expression and interpretation of Evelyn and Sayes Court should occur.
- 8.138 Where Plot 08 interfaces with Olympia Square, the public realm proposals included in the Plot P08 RMA are limited to temporary surfacing works to facilitate access around the western side of the plot. The detailed proposals for the square itself will be developed with Plot P07 to ensure coherence across this space. Re-use of existing granite setts would be appropriate within the design of this key space and should be considered at this stage of the development.
- 8.139 The preparation and use of timber in the building of the Royal fleet from Tudor times until the early 19th Century is part of the story of shipbuilding and the story of shipbuilding on this important site. The design team considered that this is best referenced through depiction in public art and objects in the urban realm such as timber benches, sculptures and friezes.
- 8.140 It is proposed that these existing timbers found on site are re-used or referenced in their scale, species and preparation to create a large-scale urban bench that will sit in the public realm on Olympia Way South adjacent to the central courtyard opening. A stair screen is positioned behind the new bench and will have timber elements incorporated into its design to further strengthen the historical timber reference. The screen is transparent to allow the soft landscaping on either side to grow through and blend the garden areas. The bench position is enhanced by its southerly aspect so it will be the ideal place to pause and enjoy the new public realm.
- 8.141 This feature would be located on an axis with the new road between Plots 15 and 16 thereby providing vistas in three directions. The sky above the bench also opens to the north with the break in building frontage for the new residential courtyard. The gate to the courtyard in the stair screen with the coming and going of residents will help the bench position to be a safe and comfortable place to be. Timber is also proposed as a material for benches and seating areas with the lifted residents' courtyard defining further the historical use and preparation of timber on this part of the site.
- 8.142 The applicant notes that the story of historical timber preparation and timber use could also be told with the use of digital interactive displays as well as in the public realm landscape and art elements. With reference to this specifically, it is noted that the S106 agreement secured a £300,000 contribution towards public art to be steered through a Public Art Strategy which would involve the local community in selecting Public Art for the development and would commission local artists to create proposals for Public Art shall reflect the special heritage of the site and the heritage of the local area.
- 8.143 An initial Public Art Strategy was submitted in April 2018, but was subsequently refused by the Council in May 2018. The applicant is currently working on a revised Public Art Strategy, which will outline how the community will be involved in the development and inclusion of public art for the site, and will reflect the aims and objectives of the Site Wide Heritage document.
- 8.144 Once approved, the Public Art Strategy will accommodate and encourage further physical manifestation and tangible reflection of the site's history in the development site.

- 8.145 The design responses to heritage have been reviewed by Historic England and the Council's Conservation Officer who are supportive of the responses proposed. It is recommended that the proposed design features as outlined above would be secured by condition. This condition would ensure that these are delivered as well as any further design features identified by the evolving Site Wide Heritage Principles.

Design Conclusion

- 8.146 The design of the proposed plot is largely constrained by the parameters of the OPP. As above, the proposed design is within the parameters and in this regard, is considered acceptable.
- 8.147 In relation to design, this reserved matter provides details of how the proposed building, landscape and public realm will appear including details of the materials to be used – this is provided alongside details of how the building would be accessed. The siting and layout is also considered in the context of the maximum and minimum approved parameters.
- 8.148 As above and in the context of the approved parameters, the layout proposed is considered optimal, maximising sunlight and daylight to the proposed units and the standard of accommodation to be provided. The layout accords with the principles of the masterplan approved within the OPP.
- 8.149 Overall, the proposed design, endorsed by the independent Design and Access Panel, provides a high quality response to the building's context within the masterplan and wider area. The proposed development is considered to have an acceptable impact with regard to the heritage assets above and below ground, both on and off the development site.
- 8.150 Acknowledging that the location of P08 does not sit upon any of the existing principal heritage assets identified, the proposed design (primarily in relation to public realm) has been amended to better reflect the site's history, and above and below ground heritage assets. The developing Site Wide Heritage Principles identifies other areas of the site more suitable in terms of location in relation to the historical and masterplan context, where heritage can be better reflected through design. These reflections through design would be captured by condition.
- 8.151 The proposed design is acceptable within the context of the OPP and is considered to reflect satisfactorily, the history and heritage of the site through design.

Other details for approval under Condition 20

Mitigation of Potential Overlooking – 20 (i)(d)

Overlooking within P08

Policy

- 8.152 NPPF para 127 sets an expectation that new development will be designed to create places that amongst other things have a 'high standard' of amenity for existing and future users.

Discussion

- 8.153 The plan of Plot 08 as informed by the OPP, as well as how the architects have articulated the floor plans to maximise dual aspect units means that there is potential for overlooking between units.
- 8.154 The two instances in which overlooking may occur are indicated in Image 11 below:

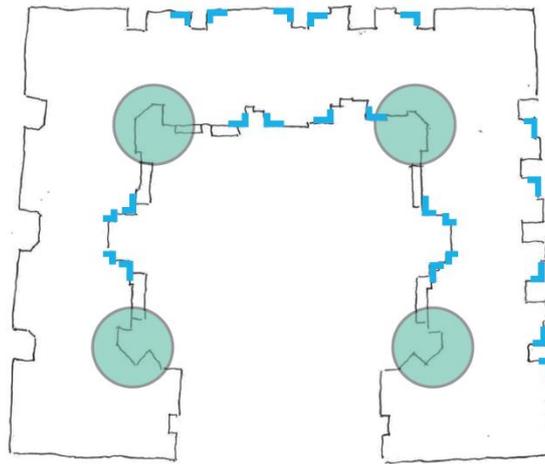


Image 11: Potential overlooking (Design and Access Statement, Farrells)

8.155 To mitigate this potential overlooking, the architects have introduced two methods of mitigation:

1. Frit obscuration on glazing (red on Image 12 below)
2. Full height screening to balconies on corner units (blue on Image 12 below)

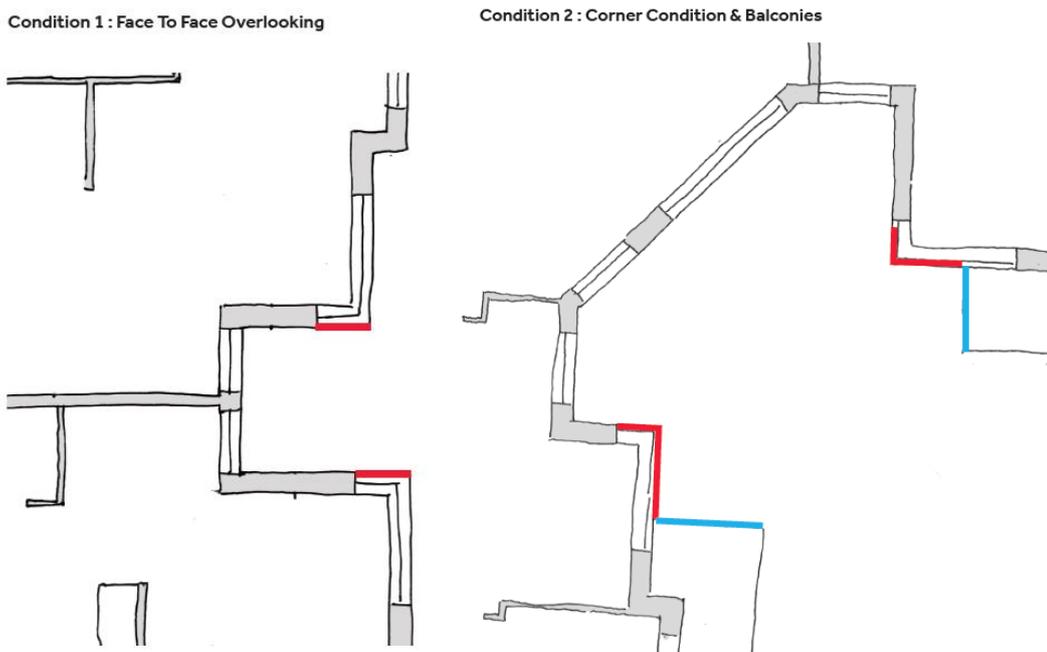


Image 12: Areas for application of obscuration and screening (Design and Access Statement, Farrells)

8.156 Given the above methods of mitigating overlooking to be employed on Plot 08, it is considered that any potential overlooking between units on Plot 08.

Overlooking to Other Development Plots

8.157 Plot 08 is located adjacent to five other development plots, which would include residential uses (P14, P15, P16, P01 and P02) as well as the Olympia Warehouse (P07).

- 8.158 The proposed building at P08 would be located a minimum of 19m (from P15) to a maximum of 27m (from P14) away from surrounding development plots. The separation distance of 19m is just below the 21m recommended by DM Policy 32, however, given the constraints presented by the OPP and the overall high quality of design, these separation distances are considered adequate and in accordance with the parameters of the OPP. Separation distances of less than 21m are commonplace in urban environments and this standard can be applied flexibly in these instances.
- 8.159 Given the above, no objections are raised with regard to potential for overlooking to other development plots.

Impact Study of Existing Water Supply – 20(i)(g)

- 8.160 Thames Water have been consulted and have no objection to the proposed development with regard to the impact on existing water supply.
- 8.161 Thames Water agree to discharge condition 20(i)(g), as the impact of the developments at Convoys Wharf site have been modelled at Outline Planning Permission stage and the findings of the report are still valid.

Details for approval under Condition 21

Infrastructure (including roads, plant and equipment) – 21(i)(a)

- 8.162 With regard to road and footway infrastructure, these have been discussed in the section on access. Additionally, as this scheme is one of the first reserved matters, the final design for the access road (spine road) including footways widths has not yet been determined, and is dependent upon the design of other Plots along the spine road being developed. As such, the final details of the spine road and footways will be determined through further Reserved Matters applications and approvals of details.
- 8.163 With regard to plant and other equipment for Plot 08, this would all be located at ground floor and first floor level adjacent to the double height retail units. Smoke vent extracts and kitchen extracts would be located on the roof at 11th storey. Further smoke vent extracts would be located on the roof of the 14th and 15th storey elements.
- 8.164 The smoke vent extracts would be installed in accordance with building regulation to allow for sufficient ventilation in the event of a fire.
- 8.165 The noise of fixed plant is controlled by condition 26 (fixed plant) of the OPP requiring fixed plant to be 5 dB below the existing background level at any time. Condition 26 further requires that a scheme demonstrating with these requirements is submitted and approved prior to commencement in the plot.
- 8.166 In relation to Condition 21(a), this requires that the details for the Spine Road, such details shall include full details of its exact location, design, dimensions, materials, any temporary access, timescales for completion and details of Spine Road bus stops and associated passenger facilities". Such details are required to be submitted not later than submission of the first Reserved Matters application for certain plots, including Plots 08. These details have not yet been provided therefore a partial discharge of condition 21(i)(a) is required in this regard.

Foul Water and Surface Water Drainage - 21(i)(b)

Policy

- 8.167 The NPPF at para 165 expects major development to incorporate sustainable urban drainage systems (SUDS) unless there is clear evidence it is inappropriate.

- 8.168 LPP 5.13 requires SUDS unless there are practical reasons for not doing so. In addition, development should aim to achieve greenfield run-off rates and ensure surface water is managed in accordance with the policy's drainage hierarchy.
- 8.169 DLPP SI13 expects development to achieve greenfield run-off rates in accordance with the sustainable drainage hierarchy.
- 8.170 CSP 10 requires applicants to demonstrate that the most sustainable urban drainage system that is reasonably practical is incorporated to reduce flood risk, improve water quality and achieve amenity and habitat benefits.

Discussion

- 8.171 The proposed development should demonstrate that the proposed form of drainage has regard to the SuDs policies as above and industry best practice.
- 8.172 Foul Water and Surface Water Drainage on the development site are regulated by Conditions 19 "Drainage and Flood Risk" and 47 "Surface Water Control Measures" of the OPP.
- 8.173 The EA have reviewed the proposed foul water and surface water drainage documents and have raised no objection with regard to the proposals.
- 8.174 The Lead Local Flood Risk Authority (LLFRA) has also been consulted. LLFRA initially requested further as follows::
- A full drainage strategy document in relation to attenuation and discharge rates
 - Confirmation of the form of detailed calculation and modelling of the proposed drainage strategy including SuDS system for the site
 - Confirmation of the greenfield runoff rate
 - Detailed calculations to support the applicant's assessment on how much storage is required
 - Consideration of modelled events and a qualitative examination of what would happen when a greater than design event occurs
 - A site-specific Maintenance Plan for Plot P08 with a clear proposed drainage strategy including SUDS elements
- 8.175 The applicant submitted the documentation/information requested, following which LLRFA have advised that the detail provided is acceptable with regard to foul water and surface water drainage and for the discharge of conditions 19 and 47 in relation to P08.

Jetty, dry dock or temporary wharf structure required for construction purposes including any works within the river - 21(i)(c)

- 8.176 This requirement is not relevant to Plot 8.

Removal of Trees - 21(i)(d)

- 8.177 No trees are proposed for removal as part of the Plot 08 proposals.

Remediation - 21(i)(e)

Policy

- 8.178 The NPPF states at para 170 that planning decisions should contribute to and enhance the natural environment by, among other things, preventing new and existing development from contributing to, being put at unacceptable risk from, or being adversely affected by, unacceptable levels of soil pollution and that development should wherever possible help

to improve local environmental conditions by remediating and mitigating contaminated land, where appropriate (para 170).

- 8.179 Further, the NPPF at para 178 and NPPG states decisions should ensure a site is suitable for its proposed use taking account of ground conditions and any risks arising from contamination and that after remediation, land should not be capable of being determined as “contaminated land” under Part 2A of the Environmental Protection Act 1990.
- 8.180 London Plan Policy 5.21 reflects national policy, whilst DM Policy 28 advises the Council will use appropriate measures to ensure that contaminated land is fully investigated.

Discussion

- 8.181 Contaminated land and remediation of each plot is further controlled by Condition 45 of the OPP which requires inter alia, the following details prior to commencement of development of each plot:
- a) Desktop study and site assessment
 - b) Site investigation report
 - c) Remediation scheme
- 8.182 The Environment Agency have reviewed the documentation provided with regard to contaminated land and have no objection in this regard.
- 8.183 The Council's Environmental Protection Officer reviewed the initial submitted documents, which originally included only a site wide remediation strategy. Following discussions with the applicant, a plot specific Desktop Study and Site Assessment, Site Investigation Report and Remediation Scheme were submitted.
- 8.184 The amended documents were reviewed by the Council's Environmental Protection Officer who considered these sufficient to satisfy Condition 21(i)(d)(remediation) as well as Condition 45.
- 8.185 Historic England have requested that the approved remediation strategy be updated following agreement of archaeological detail required under conditions 34 to 39. It is considered however, that this is a matter which should properly be assessed once the details are submitted under Conditions 34 to 39 of the OPP.

Temporary Site Boundary Treatments - 21(i)(f)

- 8.186 The proposed temporary site boundary treatments would follow the boundary outlined in Image 9 above.
- 8.187 The boundary treatment would be 2.4 metres high plywood hoarding. The hoarding would extend around the P08 plot and down both sides of the spine road to the entrance at New King Street. This is considered acceptable.
- 8.188 It is noted that the layout of temporary boundary treatment on site will evolve as other development plots come forward. Details of each plot and changes of boundary treatments to other plots would be required upon submission with details in respect of those Plots.

OTHER CONDITIONS

Internal Space Standards and Private Amenity Provision – Conditions 10 and 30

Policy

- 8.189 Standard 4.10.1 of the Mayor's Housing SPG states that 'a minimum of 5sqm of private outdoor space should be provided for 1-2 person dwellings and an extra 1sqm should be provided for each additional occupant'.
- 8.190 LPP 3.5 seeks to achieve housing development with the highest quality internally and externally in relation to their context. Minimum space standards are set out in Table 3.3 of the London Plan.

Discussion

- 8.191 Plans have been submitted under Condition 10 which show that all proposed residential meet the minimum space standards and would be provided with dedicated storage areas, which meet the minimum requirements. The proposed plans have also been annotated with essential furniture, which demonstrates that all units could comfortably accommodate the necessary furniture and circulation spaces. Internal floor to ceiling heights would be a minimum of 2.5 metres.
- 8.192 Details have also been submitted under Condition 30 which show all units would be provided with private amenity space meeting or in excess of the relevant London Plan Standards.
- 8.193 Given the above, the proposed development which meets the London Plan requirements and it is considered that adequate internal living spaces and private amenity space would be provided for the future occupiers.

Microclimate: Wind – Condition 3(ii)

- 8.194 The details submitted to discharge this Condition in respect of Plot 08 are considered below at paragraph 8.275 to 8.278.

Sunlight and Daylight to Proposed Units – Condition 4

- 8.195 Condition 4 of the OPP requires daylight and sunlight modelling to be undertaken in to inform the detailed design stage of building height and massing. The applicant has provided this information in accordance with Condition 4.

General Policy

- 8.196 NPPF para 127 sets an expectation that new development will be designed to create places that amongst other things have a 'high standard' of amenity for existing and future users.
- 8.197 Daylight and sunlight is generally measured against the Building Research Establishment (BRE) standards however, this is not formal planning guidance and should be applied flexibly according to context.
- 8.198 The NPPF does not express particular standards for daylight and sunlight. Para 123 (c) states that, where there is an existing or anticipated shortage of land for meeting identified housing need, LPAs should take a flexible approach to policies or guidance relating to daylight and sunlight when considering applications for housing, where they would otherwise inhibit making efficient use of a site.
- 8.199 Daylight is defined as being the volume of natural light that enters a building to provide satisfactory illumination of internal accommodation between sunrise and sunset. This can be known as ambient light. Sunlight refers to direct sunshine.
- 8.200 The GLA states that 'An appropriate degree of flexibility needs to be applied when using BRE guidelines to assess the daylight and sunlight impacts of new development on surrounding properties, as well as within new developments themselves. Guidelines should

be applied sensitively to higher density development, especially in opportunity areas, town centres, large sites and accessible locations, where BRE advice suggests considering the use of alternative targets. This should take into account local circumstances; the need to optimise housing capacity; and scope for the character and form of an area to change over time.' (GLA, 2017, Housing SPG, para 1.3.45).

Daylight

Discussion

- 8.201 The applicant has submitted a Daylight and Sunlight Report which demonstrates that all habitable rooms within the proposed development have been technically assessed for Average Daylight Factor (ADF), No Sky Line (NSL) and Room Depth Criterion (RDC).
- 8.202 The results show that 1343 out of the 1553 assessed rooms (87%) meet or exceed BRE's recommendations for ADF, 1114 (72%) achieve NSL levels in line with or above guidance and all rooms have been designed in accordance with the RDC where applicable.
- 8.203 With such overall compliance, the scheme is considered to perform well. Of the 210 rooms falling short of recommendation for ADF, 94 are studies, 76 are living rooms, 31 are bedrooms and three are combined living/kitchen/dining rooms. 52 of these 210 rooms, however, fall short of recommendation only by 0.1%. These are considered technical failures as only marginally below the recommended levels.
- 8.204 Most of the rooms falling short of recommendation are located in the corners of the courtyard, in recessed positions or below balconies, which typically limit the daylight availability. This is a common trade-off of different types of amenity (external amenity space vs. daylight amenity) which occurs throughout London and is generally considered acceptable.
- 8.205 As explained above, care has been taken in the positioning of projecting balconies in relation to living areas, for example by shifting them to the side of the rooms they serve, in order to leave part of the window unobstructed and thus maximise the daylight ingress. However, a degree of obstruction is unavoidable and for some of the rooms this has resulted in lower daylight levels than recommended.
- 8.206 This is particularly evident on the lowest floors or in corner/recessed positions, which have a more obstructed view of the sky and therefore see lower levels of daylights, on average, than the rest of the scheme.
- 8.207 Given the excellent sunlight availability discussed in below, however, it is noted that most rooms will appear much brighter on sunny days.

Sunlight

Policy

- 8.208 The Annual Probable Sunlight Hours (APSH) relates to sunlight to windows. BRE guidance states that a window facing within 90 degrees due south (windows with other orientations do not need assessment) receives adequate sunlight if it receives 25% of APSH including at least 5% of annual probable hours during the winter months.

Discussion

- 8.209 The results of the assessment show a good sunlight availability, with the majority of assessed rooms receiving very good levels of sunlight throughout the year.

- 8.210 Levels of APSH lower than those suggested for the whole year can be found in the living areas located below balconies on the lowest floors and in the corners within the courtyard. However, the sunlight intercepted is transferred to the balconies and therefore future occupants will be able to enjoy it through the use of their private amenity spaces during the summer. As explained above in relation to the daylight levels, a trade-off of different types of amenity is generally considered acceptable where balconies are provided.
- 8.211 The sunlight availability during the winter months (WPSH) is very good, as the balconies cause less obstruction to direct sunlight when the sun is lower in the sky.
- 8.212 With very good levels of sunlight enjoyed in the majority of assessed living areas and, alternatively, on their balconies, the proposed scheme is considered to offer very good sunlight amenity.

Overshadowing

Policy

- 8.213 BRE states that in order for a public or communal amenity space to be well sunlit, at least 50% of its area should receive direct sunlight for two or more hours on 21st March.

Discussion

- 8.214 An assessment of Sun Hours on Ground has been undertaken on the amenity area at podium level in order to ascertain compliance with this criterion. The result shows that the courtyard will see well above the minimum recommended, with 77% of the area seeing at least two hours of sunlight on the spring equinox.
- 8.215 In order to illustrate the sunlight availability throughout the year, sun exposure assessments have also been undertaken for the equinoxes and summer solstice. These show that during the equinoxes most of the area will in fact see around three hours of direct sunlight, whilst in summer, when outdoor spaces are more likely to be used, three to six hours of direct sunlight will be available on most of the area.
- 8.216 It can therefore be concluded that the proposed communal amenity area within the site will offer very good levels of sunlight throughout the year.

Sunlight and Daylight Conclusion

- 8.217 The Sunlight and Daylight Assessment provided with the application demonstrates that the proposed development would provide a good degree of daylight and sunlight to the proposed units, and that the proposed communal area at podium level would not be subject to an unreasonable degree of overshadowing.
- 8.218 Whilst some of the BRE guidelines are not fully complied with regard to daylight, the proposed units would receive good levels of sunlight throughout the year. It is noted that the non-compliant units are largely as a result of the parameters set at outline stage and due to the provision of balconies in order to comply with the relevant private external space standards.
- 8.219 Given the above, it is considered that the proposals are acceptable with regard to daylight, sunlight and overshadowing.

TRANSPORT IMPACTS

Servicing, Delivery and Waste Management

- 8.220 Transport for London and LBL Highways have requested that a condition should be attached to the Reserved Matters approval requiring a servicing, delivery or waste management plan to be submitted and approved. The traffic impacts were assessed at OPP stage and but it was not considered necessary to impose such a condition on the OPP. In any event, servicing of Plot 08 would occur on roads within the development site and Officers that there is unlikely to be any unreasonable impact on the existing road network. In the circumstances, the suggested condition is not considered to be reasonable or appropriate, where there are no changes in surrounding context.

Vehicular and Cycle Parking – Conditions 25 and 33

Outline Consent Background

- 8.221 The Outline Planning Consent secured a maximum quantum of 1,840 car parking spaces across the whole site. The development will provide 1540 spaces for residents and 300 car parking spaces for the remaining, non-residential components of the developments, including up to 35 car club spaces within the non-residential provision. These spaces will be provided principally at ground level across much of the site and first floor parking decks beneath landscaped podiums.

Residential Parking

- 8.222 The proposals for P08 include 182 residential car parking spaces to be provided within the internal podium at ground and first floor level.
- 8.223 The proposed provision of residential parking spaces is in accordance with the Outline Planning Permission and is proportionate to the quantum of residential units to be provided in the context of the approved OPP parking numbers. Furthermore, the proposals have been reviewed by Transport for London and the Council's Highways Officer who have raised no objection.
- 8.224 Given the above, the proposed residential parking is considered acceptable. It is noted that the applicant is also required to provide prior to commencement, a Car Parking Management Strategy under condition 31 of the OPP. This has not been submitted as part of this application, but references made in this report for completeness. Full details of ventilation to basement parking have not been submitted at this stage and would be required prior to commencement of works through condition 25 of the OPP in respect of the basement and semi-basement parking areas.

Accessible Parking

- 8.225 Planning Policy and the approved S106 agreement require that each wheelchair unit is allocated a parking space. As outlined above, 46 of the 456 units across Plot 8 would be provided in accordance with Building Regulations requirement M4(3) 'wheelchair user dwellings'
- 8.226 The parking provision within the podium includes 46 accessible spaces (20 spaces at ground and 26 spaces at first floor level) meaning that accessible parking is provided at a 1:1 ratio as required. The spaces would have level access to lifts to each of the proposed wheelchair units.
- 8.227 Given the above, the proposals are in accordance with the OPP.

On-street Parking

- 8.228 The proposals for P08 include 14 non-residential spaces, two of which will be designated for use by mobility impaired persons, to be located at street level. These would primarily be located along 'Royal Caroline Square' to the south-eastern elevation of the block.

- 8.229 This quantum is in accordance with the Outline Planning Permission, which has provision for 300 on-street non-residential spaces to be provided at street level. The proposals for on-street parking have been reviewed by Transport for London and the Council's Highways Officer who have raised no objections, and are in accordance with the Outline Planning Permission. As such, the proposals are acceptable in this regard.
- 8.230 Further on-street parking to the north-western elevation of the proposed development block, along 'Olympia Square' would be provided as future development plots are brought forward. The on-street arrangements in this location as currently proposed are temporary as outlined in Image 6.

Car Club Provision

- 8.231 The OPP consented up to 35 car club spaces within the non-residential provision (300 spaces). These spaces were outlined as being provided principally at ground level across much of the site and first floor parking decks beneath landscaped podiums.
- 8.232 Plot 08 would provide two on-street car club spaces to the north-eastern elevation of the proposed block on 'Olympia Way North'. Two further bays have been identified on 'Royal Caroline Square', which could be converted from standard on-street non-residential parking bays to car club spaces subject to future demand. The proposals for car club provision have been reviewed by Transport for London and the Council's Highways Officer who have raised no objections, and are in accordance with the Outline Planning Permission. As such, the proposals are acceptable in this regard.

Electric Vehicle Charging Points – Condition 50

Policy

- 8.233 LPP 6.3 (Parking) requires that 1 in 5 spaces are provided as Electric Vehicle Charging Points (EVCPs). Draft London Plan Policy requires that at least 20% of parking spaces are provided as EVCPs.

Discussion

- 8.234 The details for provision and maintenance of EVCPs are required to be approved prior to commencement and the applicant is now seeking such approval as part of this application.
- 8.235 The original submission proposed 42 EVCPs within the residential parking. This resulted in 23% of the proposed residential spaces being EVCPs, however no EVCP provision was provided for the 46 accessible parking spaces. Following consultation with TfL, 11 of the EVCPs were incorporated into accessible parking spaces.
- 8.236 Of the 14 non-residential on-street parking spaces, 5 of these would be provided as EVCPs equating to a percentage of 35%.
- 8.237 Given the above, the EVCP provision is considered policy compliant and acceptable. Furthermore, condition 50 can be discharged in relation to Plot 08.

Cycle Parking – Condition 33

- 8.238 Following consultation with TfL, the cycle parking was amended to meet the London Cycle Design Standards in order to provide an element of accessible spaces and standard Sheffield stands. As a result, a total of 816 cycle parking spaces are provided as part of the scheme as follows:
- 776 internal long-stay spaces for residents including;
 - 40 accessible Sheffield stand bays

- 170 standard Sheffield stand bays
- 566 two tier stand bays
- Eight internal short-stay spaces for visitors/residents;
- Four external short-stay spaces for visitors/residents;
- 28 external short-stay spaces for customers

8.239 The proposed cycle parking has been amended following consultation with by Transport of London and the Council's Highways Officer to conform with London Cycle Design Standards. Following amendment, these details are considered acceptable for discharge of condition 33 in relation to P08.

Healthy Streets

Policy

8.240 The Healthy Streets Approach puts people and their health at the centre of decisions about how we design, manage and use public spaces. It aims to make our streets healthy, safe and welcoming for everyone.

8.241 The Approach is based on 10 Indicators of a Healthy Street which focus on the experience of people using streets.

8.242 Policy T2 (Healthy Streets) of the Draft London Plan states Development proposals should:

- 1) demonstrate how they will deliver improvements that support the ten Healthy Streets Indicators in line with Transport for London guidance.
- 2) reduce the dominance of vehicles on London's streets whether stationary or moving.
- 3) be permeable by foot and cycle and connect to local walking and cycling networks as well as public transport.

Discussion

8.243 A Healthy Streets Assessment has been submitted in support of the application to demonstrate how most links in the existing road network responds adequately to the standards set by the Healthy Streets assessment, based on current traffic flows, pedestrian and cycle flows, mix of land uses.

8.244 The assessment of the urban design proposals for the streets adjacent to Plot 08 has also shown how Convoys Wharf development and the streetscape improvements proposed as part of the Reserved Matters Application will align with the Healthy Streets principles. As such, the proposals would contribute to improving pedestrian and cycle permeability, road safety and street amenity both within the development's internal street network, and along some of the routes that will connect to the site.

8.245 It is noted that improvements to the street network are currently being trialled in the area. A small eastern section of Prince Street (the section east of New King Street) has currently been closed to vehicles and available only for cycle and pedestrian traffic.

8.246 Where some improvements to the existing street network (outside of the application site) have been identified outside the application site, it is considered that there is potential for contributions secured for highways improvements within the Section 106 agreement to be diverted towards these areas when the contributions are released in accordance with the triggers for payment outlined in the S106 agreement.

Code of Construction Practice – Condition 44(ii)

- 8.247 Condition 44(i) of the Outline Planning Permission requires that a site-wide Code of Construction Practice be submitted prior to any development to establish the overarching principles of best construction practice, and is to be based on the Framework Code of Construction Practice, 14 February 2014 (Appendix C of Environmental Statement Addendum Report), as approved by the OPP.
- 8.248 Further to the above, Condition 44(ii) of the OPP requires that prior to commencement of development on a particular plot, a plot-specific Code of Construction Practice be submitted.
- 8.249 A draft Code of Construction Practice has been provided with this application for approval under condition 44(ii) of the OPP. The Council's Highways Officer has advised that these details are generic and not sufficiently specific to the plot and cannot be discharged at this time. It is therefore recommended that the CoCP is not approved under Condition 44(ii). As such, this will remain a requirement to be discharged prior to commencement in Plot 08.

SUSTAINABILITY AND ENERGY – Condition 15

Policy and Outline Consent Background

- 8.250 Policy 5.3 Sustainable Design and Construction of the London Plan states that the highest standards of sustainable design and construction should be achieved in London to improve the environmental performance of new developments and to adapt to the effects of climate change over their lifetime.
- 8.251 Policy 5.2 Minimising Carbon Dioxide Emissions of the London Plan states that development should make the fullest contribution to minimising carbon dioxide emissions in accordance with the following energy hierarchy:
1. Be lean: use less energy
 2. Be clean: supply energy efficiently
 3. Be green: use renewable energy
- 8.252 Achieving more sustainable patterns of development and environmentally sustainable buildings is a key objective of national, regional and local planning policy. London Plan and Core Strategy Policies advocate the need for sustainable development. All new development should address climate change and reduce carbon emissions. Core Strategy Policies advocate the need for sustainable development. All new development should address climate change and reduce carbon emissions.
- 8.253 The Section 106 agreement required that the owner submit and have approved an 'Interim Energy Strategy' prior to the submission of the first reserved matters application. The intention of the Interim Energy Strategy is to demonstrate how the applicant would secure a connection from the development to the off-site South East London Combined Heat and Power plant (SELCHP). The Interim Energy Strategy was submitted to the Local Planning Authority prior to the first Reserved Matters submission and following amendments was approved on 10th January 2017.
- 8.254 The Section 106 agreement also required that the applicant, on submission of the first Reserved Matters Application shall submit the Energy Strategy to the Council for approval and shall:
- “accompany the Energy Strategy with a written statement addressing how the steps required by the Interim Energy Strategy are being addressed and if the connection to SELCHP has not been secured, the Energy Strategy shall include an explanation as to why the connection has not been possible, how any obstacles are proposed to be addressed*

through Phase 1 and subsequent Phases of the Development and the further strategy for securing the connection to SELCHP.”

Discussion

- 8.255 The applicant has submitted an Energy and Sustainability Statement, which follows the overall strategy set out in the approved Interim Energy Strategy (RPT-0003).
- 8.256 The Energy and Sustainability Statement states that baseline energy demand for the development would be reduced by using energy efficiency measures and passive design, prior to the inclusion of appropriate low and zero carbon energy technologies, since limiting the demand is the most effective way of reducing overall carbon emissions.
- 8.257 Carbon reduction would be further achieved by the implementation of Combined Heat and Power (CHP) using one of two potential options. Option 1 comprises a connection to the off-site South East London Combined Heat and Power plant (SELCHP). This option is expected to deliver approximately 27% lower carbon emissions than a Part L 2010 compliant baseline development, or 45% lower emissions, if regulated loads are assessed. This option is subject to commercial negotiations with Veolia, the operator of SELCHP, which are ongoing. If such connection to SELCHP is not found to be viable then the alternative option is to provide onsite Energy Centres, which will be gas-fired CHP with gas-fired boilers supplementary to meet peak loads. Under this scenario the development is expected to achieve CO₂ emissions reductions of approximately 11% lower than Part L 2010 standards, or approximately 23% lower than Part L 2010 base load calculations with a 2% renewable contribution. As Option 1 remains a viable option, this is considered acceptable to comply with condition 15 in relation to P08.
- 8.258 It should be noted that if the SELCHP connection is not ready or determined viable by the time the first phase of redevelopment is occupied the on-site district heating network would still allow a future connection to SELCHP to be made, should it prove viable or available at a later stage.
- 8.259 The technical and financial feasibility of finding a route for the pipework will require that the underground services be mapped of the identified connection routes. These will then be analysed, and the least disruptive route selected. Discussions will then be held with utility providers to determine the costs and timescales of any diversions required to allow the connection to proceed.
- 8.260 The applicant and Veolia (who own and operate SELCHP) entered into a Pre-Development Agreement in November 2016 to commence a feasibility study for the pipe route between SELCHP and Convoys Wharf. Since this time, Veolia have been working on the pipework feasibility study between SELCHP and Convoys Wharf.
- 8.261 Veolia identified and analysed a number of different pipe route and selected a preferred pipe route as part of their initial study.
- 8.262 In 2017, Veolia’s team presented their initial proposals to the Lewisham Council (Planning and Sustainability Services) and concerns were raised regarding some of the routing of the pipes, due to third party land ownership issues in particular. Since then, Veolia have been pursuing this initial route and trying to overcome the legal issues caused by a route involving third party land ownership.
- 8.263 In 2020, Veolia were awarded £5.5million funding through the central government Heat Network Investment Programme (HNIP) to initiate a heat network in Lewisham through a connection to Convoys Wharf. This funding is awarded on a conditional basis and is dependent on Convoys Wharf coming forward. The Council is working with Veolia to support the development of this heat network to establish a Strategic Heat Network for the borough.

8.264 Whilst the connection to SELCHP has not yet been formally secured, it is considered that the applicant has demonstrated ongoing progress in this regard and that the connection is being pursued. The strategic heat network remains critical to the Council in delivering a source of low carbon heating and forms an action point in the Climate Emergency Action Plan (2020).

ENVIRONMENTAL CONSIDERATIONS

8.265 An Environmental Impact Assessment (EIA) Statement of Conformity (SoC) has been submitted with this reserved matters application. The SoC assesses whether the detailed scheme presented in the current application will give rise to new or materially different likely significant effects on the environment from those considered as part of the outline planning permission and thus whether the reserved matters are required to be subject to environmental impact assessment under the EIA Regulations.

8.266 As set out below, it is considered that there are no new or materially different likely significant effects on the environment from those identified in Environmental Statement (April 2013) and a Supplementary Environmental Statement (February 2014) which set out the environmental effects of the outline planning permission based on an assessment of the Approved Parameters. As such, an EIA is not required in relation to the proposals set out in the reserved matters application.

8.267 The topics assessed within the Approved Environmental Statement, submitted in support of the Outline Planning Permission, are as follows:

- Archaeology;
- Built Heritage Assessment;
- Landscape, Townscape and Visual Amenity Assessment;
- Air Quality Assessment;
- Soils, Ground Conditions and Groundwater Quality Assessment;
- Ecological Impact Assessment;
- Noise and Vibration Assessment;
- Socio economic Assessment;
- Sunlight, Daylight and Overshadowing Assessment;
- Electronic Interference Assessment;
- Traffic and Transport Assessment;
- Waste Management Assessment;
- Water Resources including Flood Risk Assessment; and
- Wind and Microclimate Assessment.

8.268 The proposals sought within the P08 Reserved Matters Application will not be materially different from the Outline Planning Permission Approved Parameters assessed within the Approved Environmental Statement. With the exception of the minor breach of the minimum parameter at podium level, which has been approved as a non-material amendment, the P08 Proposals are within the Approved Parameters and design guidelines approved within the OPP.

8.269 Given the above, the majority of the technical topics assessed in the Approved Environmental Statement would therefore, not be affected by the P08 Proposals and the information in the Approved Environmental Statement remain valid. However, due to the changes to massing resulting from the 1100mm 'breach' of the Minimum Development Plot Parameters at podium level of P08 (to improve sunlight conditions), further consideration has been given to the potential for additional or different environmental effects arising from the following technical topics:

- Wind and Micro-climate; and
- Sunlight, Daylight and Overshadowing.

Wind Microclimate

- 8.270 A wind microclimate assessment has been submitted for P08 to discharge Condition 3(ii) of the OPP (assessed in the Discharge of Conditions section of this report below). Overall, the results show that, following development, most areas of the pedestrian level, balconies, and rooftop terraces of Plot 08, and surrounding areas, are acceptable for the proposed uses, although several locations are identified where some minor or localised mitigation may be required, either in the form of adjustment of proposed usage, or provision of localised shelter (such as through the installation of porous fencing/structures, evergreen planting or other measures). The wind microclimate report submitted in support of the Plot 08 Reserved Matters Application includes wind mitigation (including provision of localised shelter such as porous fencing/structures, evergreen planting) to ensure that the wind conditions for Plot 08 and the immediate surrounds would be similar or an improvement upon those reported in the Approved Environmental Statement.
- 8.271 The design change to include lower parapets on the tower roofs of the Plot 08 building results in areas on the edges of the tower roofs being exposed to higher winds than observed in the previous assessment presented in the Approved Environmental Statement. As a consequence some small areas on the edge of the roof are unsuitable for pedestrians to walk on. This can be avoided by not allowing the roof to be used on windy days, or consideration of mitigation measures such as slightly increasing the height of the parapets, or the addition of some evergreen planting or porous screens around the edge. In any event, these areas of roof would accommodate brown living roof and would not be available for public access – only very limited accessibility would be required for these areas to allow for maintenance or similar activities to occur.
- 8.272 Following implementation of the recommended mitigation, it is considered that the residual effects and conclusions of the Approved Environmental Statement in relation to wind microclimate represent a worse case and remain valid. Temple Group were commissioned by the Planning Service to conduct a review of the wind microclimate assessment in relation to Condition 3(ii) (Microclimate: wind) of the OPP. They concluded that the proposals were acceptable with regard to microclimate, subject to:
- Screens or planting being installed on terrace region 2 (terrace region 2 is the roof terrace serving flat 11.2.01, core 2 on floor 11)
 - Rooftop areas where there are exceedances of the distress and safety criteria not being accessed by the public.
- 8.273 The following mitigation measures would be imposed by condition. As such, the proposals are acceptable in this regard.

Sunlight, Daylight and Overshadowing

- 8.274 Localised daylight and sunlight conditions and overshadowing assessments for the amenity of future residents within the proposed development have been submitted in support of the Plot 08 Reserved Matters Application.
- 8.275 Within the Approved Environmental Statement, average daylight factor (ADF) results were calculated for various individual units within the proposed building plots. The 1st floor apartments within Plot 08 were expected to receive the recommended ADF. The updated results for Plot 08 as presented in the current Reserved Matters Application show there will be no noticeable change in the levels of daylight and sunlight experienced within the proposed residential units of Plot 08 compared to those identified for Plot 08 in the Approved Environmental Statement, which showed that typical 1st floor apartments in Plot 08 were expected to receive the recommended ADF.

- 8.276 Additionally, with regard to overshadowing to the courtyard within Plot 08, there will be a slight improvement in relation to the amount of direct sunlight received by the amenity space owing to the detailed design of P08 presented in the Reserved Matters Application. P08 would therefore continue to pass the Building Research Establishment (BRE) recommendations for levels of sunlight to amenity areas.
- 8.277 Given that the maximum building height and massing parameters are not breached within Plot 08, and due to the location of P08 within the OPP, there would not be any different or additional effects to external receptors or other development plots within the site. As such, it is considered that the residual effects and conclusions of the Approved Environmental Statement in relation to sunlight, daylight and overshadowing remain valid.

Conclusion

- 8.278 It is concluded that that the development will not give rise to new or materially different effects from those previously identified and that the mitigation identified in the in Environmental Statement (April 2013) and a Supplementary Environmental Statement (February 2014) remains appropriate. This mitigation is secured through conditions attached to the OPP. A further condition is proposed to be added to the Reserved Matters Approval in relation to Terrace Region 2 and the rooftop areas as referred to above.

NATURAL ENVIRONMENT

General Policy

- 8.279 Contributing to conserving and enhancing the natural environment and reducing pollution is a core principle for planning.
- 8.280 The NPPF and NPPG promote the conservation and enhancement of the natural environment (chapter 15) and set out several principles to support those objectives.
- 8.281 The NPPF at para 180 states decisions should ensure that new development is appropriate for its location taking into account the likely effects (including cumulative effects) of pollution on health, living conditions and the natural environment, as well as the sensitivity of the site or wider area to impacts that could arise from the development.
- 8.282 London Plan Policy 2.18 sets out the Mayor of London's vision for Green Infrastructure as a multifunctional network that brings a wide range of benefits including among other things biodiversity, adapting to climate change, water management and individual and community health and well-being.

Ecology and Biodiversity including Green and Brown Roofs – Condition 14

Policy

- 8.283 Section 40 of the Natural Environment and Rural Communities Act 2006 places a duty on all public authorities in England and Wales to have regard to the purpose of conserving biodiversity.
- 8.284 The NPPF at para 170 states decisions should minimise impacts on and provide net gains for biodiversity, including by establishing coherent ecological networks that are more resilient to current and future pressures. At para 175, it sets out principles which LPAs should apply when determining applications in respect of biodiversity.
- 8.285 London Plan Policy 7.19 seeks wherever possible to ensure that development makes a positive contribution to the protection, enhancement, creation and management of biodiversity.

- 8.286 LPP 5.11 encourages major development to include planting and especially green roofs and walls where feasible, to deliver as many of the policy's seven objectives as possible.
- 8.287 DLPP G5 expects major development to incorporate measures such as high-quality landscaping (including trees), green roofs and green walls.
- 8.288 CSP 7 expects urban greening and living roofs as part of tackling and adapting to climate change. DMP 24 requires all new development to take full account of biodiversity and sets standards for living roofs.

Discussion

- 8.289 The Development Specification as approved under the OPP requires that Compensatory habitat, in the form of bio-diverse roofs or at ground level, will be the same or greater than the area of lost habitats, which equates 18,300sqm, approximately 11%. This is controlled by condition 14 of the OPP.
- 8.290 It is proposed that Plot 08 would provide 1187sqm of biodiverse green roof and 972 sqm of biodiverse brown roof. Cross sections have been provided of both roof types. This would cover the majority of roofspace available to the proposed building.
- 8.291 The proposed green and brown roofs to Plot 08 are considered to be policy compliant and in accordance with the OPP and that the details submitted under Condition 14 should be approved in relation to Plot 08.

Lighting – Condition 12

Outline Consent Background

- 8.292 Condition 12 of the OPP requires that at the same time as the first Reserved Matters application is submitted, a lighting strategy for external lighting across the site, including details of a dark corridor, shall be submitted to the Local Planning Authority.

Discussion

- 8.293 As part of this submission, the first Reserved Matters Application, the applicant has submitted a Site Wide Lighting Strategy. The applicant has submitted a high level lighting strategy for the development site in accordance with Condition 12 (i) of the Outline Planning Permission. No plot specific lighting strategy has been submitted under Condition 12(ii), but such strategy is required within 6 months following commencement within the relevant Plot.
- 8.294 The Site Wide Lighting Strategy has divided the development site into three different lighting zones, along with the creation and maintenance of a dark corridor along the river frontage.
- 8.295 The level of light required in each public area has been selected depending on the use for that particular area. The lighting classes have been taken from the relevant British Standards.
- 8.296 The Council's Ecology and Highways Teams have reviewed the proposed Site Wide Lighting Strategy and have raised no objection to the detail provided. It is recommended that the strategy be approved under Condition 12(i).

Air Quality

Policy

- 8.297 The NPPF at para 170 states decisions should among other things prevent new and existing development from contributing to, being put at unacceptable risk from, or being adversely affected by, unacceptable levels of air pollution. Development should, wherever possible, help to improve local environmental conditions such as air quality.
- 8.298 Proposals should be designed and built to improve local air quality and reduce the extent to which the public are exposed to poor air quality. Poor air quality affects people's living conditions in terms of health and well-being. People such as children or older people are particularly vulnerable.
- 8.299 London Plan Policy 7.14 states new development amongst other requirements must endeavour to maintain the best ambient air quality (air quality neutral) and not cause new exceedances of legal air quality standards. Draft London Plan SI1 echoes this.
- 8.300 Further guidance is given in the Mayor of London's Air Quality Strategy.

Discussion

- 8.301 A number of representations from the public raise Air Quality as a concern. The Environmental Impact Assessment SoC in respect of the application does not identify any new or materially different likely effects resulting from the development compared to those considered at the OPP stage. The impacts arising in respect of air quality were considered at OPP Stage and addressed through the Section 106 Agreement, which secures £100,000 towards for air quality monitoring in respect of the development. Officers therefore consider that appropriate mitigation and monitoring has already been secured through the OPP.

Flood Risk

Policy

- 8.302 Paragraph 155 of the NPPF (2019) requires new development to be sited away from areas at risk of flooding, whilst para.165 states that major development should incorporate sustainable drainage systems, unless there is clear evidence that this would be inappropriate.
- 8.303 London Plan Policy 5.12 requires the mitigation of flooding, or in the case of managed flooding, the stability of buildings, the protection of essential utilities and the quick recovery from flooding.
- 8.304 London Plan and draft London Plan Policies 5.12 and 5.13 requires new development proposals to comply with the flood risk assessment and management requirements set out in the NPPF. London Plan Policy 7.13 expects development to contribute to safety, security and resilience to emergency, including flooding.
- 8.305 Core Strategy Policy 10 requires developments to result in a positive reduction in flooding to the Borough
- 8.306 The site is located in Flood Zone 3 which is defined as having a 'high probability' of river and sea flooding by the 'flood risk and coastal change' section of the national Planning Practice Guidance.

Discussion

- 8.307 The OPP was submitted with a Flood Risk Assessment which was assessed and approved. This document set out the framework for flood risk management in relation to the proposed development. Various conditions are relevant to this framework as follows:

- Condition 6 (River Wall Surveys) – submitted and approved by the Local Planning Authority under planning application reference DC/17/100954 on 21 June 2018
- Condition 14 (Biodiversity) – Assessed and details recommended for approval in ‘Ecology and Biodiversity including Green and Brown Roofs’ above
- Condition 16 (River Wall Safeguarding) – not relevant to Plot 08
- Condition 19 (Drainage and Flood Risk) – relevant to this application
- Condition 47 (Surface Water Control Measures) – relevant to this application
- Condition 52 (Tidal Flood Defence) – This condition is not sought for discharge in this application
- Condition 66 (Hydrology and Water Resources) – Compliance only

8.308 The Environment Agency have reviewed the Reserved Matters Application and have raised no objections with regard to Flood Risk. The Council’s Flood Risk Manager has reviewed the application and requested further information in relation to Condition 19 (Drainage and Flood Risk) and Condition 47 (Surface Water Control Measures). Following receipt of this information it was considered appropriate to discharge conditions 19 and 47 in relation to Plot 08.

8.309 Given the above, the proposed development is acceptable with regard to flood risk and conditions 19 and 47 can be discharged in relation to Plot 08.

Building Regulations

8.310 Concerns have been raised by representees objecting to the proposed development in relation to maximum distances to protected fire escapes being exceeded by the proposed development in that some units/areas would be located in excess of 15m from the escapes.

8.311 The applicant has provided the following response to these concerns:

8.312 “Fire safety aspects of the design of this building are addressed in Building Regulations. Part B of Schedule 1 of the Regulations sets out five fire safety requirements that have to be satisfied. These requirements are ‘functional’ and set out performance requirements that have to be satisfied. For example, Requirement B1 requires that the building be designed and constructed so that there are appropriate provisions for early warning of fire and appropriate means of escape in case of fire. To assist designers in what is considered appropriate various guidance documents have been produced, such as the Approved Documents produced by HM Government, and other various British, European, and International Standards.

8.313 None of these guidance documents contain mandatory clauses or prescriptive requirements, and it is acceptable to develop alternative solutions from the recommendations made, provided such alternative designs are supported by adequate evidence that the functional requirements of the Building Regulations will be met. For Convoy’s Wharf Plot 8, we have chosen to adopt BS 9991: Fire safety in the design, management and use of residential buildings – Code of Practice 2015 as the basis for the design. This document would recommend that single direction travel distances from an apartment entrance door, to a storey exit is limited to a maximum 15 m. This is on the basis that sprinklers are installed within the apartments, and smoke ventilation is provided to the corridor (the ventilation in this instance could be by way of a single natural smoke shaft).

8.314 In order to satisfy the client brief, the single travel distances are exceeded with a maximum distance of 25m in some instances. To address this, an alternative approach has been adopted, based on fire engineering, and adopts a higher level of performance for the smoke ventilation system in the corridors for both means of escape and fire-fighting. This is a mechanical smoke ventilation system that is designed to provide an air change rate within the corridor and operates automatically on detection of smoke in the corridor. The performance of this system is significantly higher than the recommended solution within BS

9991, and has been designed based on the guidance contained within Guidance on smoke control to common escape routes in apartment buildings (flats and maisonettes) produced by the Smoke Control Association. Subsequent modelling of the system using Computational Fluid Dynamics (CFD) is also being undertaken as part of the approvals process under the Building Regulations. As part of this approvals process, there will also be a consultation required with the Fire Authority as the building will be subject to the requirements of the Regulatory Reform (Fire Safety) Order 2005 once the building is complete and occupied.”

8.315 The applicant has outlined above how they intend to comply with the relevant building regulations - this is governed by legislation separate to planning legislation. The applicant is required to meet the relevant standards under building regulations or be subject to appropriate enforcement action under this legislation – an informative is added to this effect.

9.0 SUMMARY REGARDING DISCHARGE OF CONDITIONS

9.1 In addition to the reserved matters and other details required by Condition 20, the applicant seeks to discharge a number of conditions attached to the OPP. The additional conditions sought for discharge are set out in Table 11 below, along with the assessment. The full wording of the conditions is laid out in the outline planning permission decision notice, attached as Appendix 1.

Condition	Assessment
3. Microclimate: wind (ii)	Acceptable – assessed in “Environmental Impact Considerations – Microclimate”
7. Building design Statement and Tall Buildings Design Statement	Acceptable – the application has been submitted with a Building Design Statement in relation to P08 outlining how the Design Guideline in CW04 have been applied to the proposed development
8. Reconciliation Statement (i)	Acceptable – the application has been submitted with a reconciliation statement as required by condition 8(i)
12. Lighting (i)	Acceptable – assessed in “Natural Environment – Lighting”. Site-wide only.
13. Heritage Statement	Acceptable – assessed in “Impact of Design on Heritage Assets”
14. Biodiversity (i)	Acceptable – assessed in “Natural Environment - Ecology and Biodiversity including Green and Brown Roofs”
15. Energy Statement	Acceptable – assessed in “Energy and Sustainability” above
19. Drainage and flood risk	Acceptable – assessed in “21(i)(b) – Foul Water and Surface Water Drainage”
21. Details relating to infrastructure and other matters	21(b),(c),(d),(e) and (f) acceptable – assessed in “Details for approval under Condition 21”. Partial discharge of condition 21(i)(a) as details of bus stops not provided
33. Details of cycle parking	Acceptable – assessed in “Transport Impacts - Vehicular and Cycle Parking”
44. Code of Construction Practice	Not acceptable
45. Contaminated Land (i)	Acceptable – assessed in “21(i)(e) – Remediation”
47. Surface water control measures	Acceptable – assessed in “21(i)(b) – Foul Water and Surface Water Drainage”
50. Electric vehicle charging points (i)	Acceptable – assessed in “Transport Impacts – Electric Vehicle Charging Points”

Table 11: Conditions sought for discharge and assessment

9.2 Given the above, the following conditions 3(ii), 7, 8, 13, 14, 15, 19, 21(b), (c),(d),(e) and (f), 33, 45(i), 47 and 50(i) are recommended for discharge so far as relates to Plot 08. 21(a) is

recommended for partial discharge in relation to P08 as details of bus stops are yet to be provided. The site-wide lighting strategy submitted under Condition 12(i) is also recommended for approval.

10.0 EQUALITIES CONSIDERATIONS

10.1 The Equality Act 2010 (the Act) introduced a new public sector equality duty (the equality duty or the duty). It covers the following nine protected characteristics: age, disability, gender reassignment, marriage and civil partnership, pregnancy and maternity, race, religion or belief, sex and sexual orientation.

10.2 In summary, the Council must, in the exercise of its function, have due regard to the need to:

- a) eliminate unlawful discrimination, harassment and victimisation and other conduct prohibited by the Act;
- b) advance equality of opportunity between people who share a protected characteristic and those who do not;
- c) foster good relations between people who share a protected characteristic and persons who do not share it.

10.3 The duty continues to be a “have regard duty”, and the weight to be attached to it is a matter for the decision maker, bearing in mind the issues of relevance and proportionality. It is not an absolute requirement to eliminate unlawful discrimination, advance equality of opportunity or foster good relations.

10.4 The Equality and Human Rights Commission has recently issued Technical Guidance on the Public Sector Equality Duty and statutory guidance entitled “Equality Act 2010 Services, Public Functions & Associations Statutory Code of Practice”. The Council must have regard to the statutory code in so far as it relates to the duty and attention is drawn to Chapter 11 which deals particularly with the equality duty. The Technical Guidance also covers what public authorities should do to meet the duty. This includes steps that are legally required, as well as recommended actions. The guidance does not have statutory force but nonetheless regard should be had to it, as failure to do so without compelling reason would be of evidential value. The statutory code and the technical guidance can be found at:

<https://www.equalityhumanrights.com/en/publication-download/technical-guidance-public-sector-equality-duty-england>

10.5 The Equality and Human Rights Commission (EHRC) has previously issued five guides for public authorities in England giving advice on the equality duty:

1. The essential guide to the public sector equality duty
2. Meeting the equality duty in policy and decision-making
3. Engagement and the equality duty
4. Equality objectives and the equality duty
5. Equality information and the equality duty

10.6 The essential guide provides an overview of the equality duty requirements including the general equality duty, the specific duties and who they apply to. It covers what public authorities should do to meet the duty including steps that are legally required, as well as recommended actions. The other four documents provide more detailed guidance on key areas and advice on good practice. Further information and resources are available at:

<https://www.equalityhumanrights.com/en/advice-and-guidance/public-sector-equality-duty-guidance>

10.7 The planning issues set out above do not include any factors that relate specifically to any of the equalities categories set out in the Act, and therefore it has been concluded that no impact on equality.

11.0 HUMAN RIGHTS IMPLICATIONS

11.1 In determining this application the Council is required to have regard to the provisions of the Human Rights Act 1998. Section 6 of the Human Rights Act 1998 prohibits authorities (including the Council as local planning authority) from acting in a way which is incompatible with the European Convention on Human Rights. "Convention" here means the European Convention on Human Rights, certain parts of which were incorporated into English law under the Human Rights Act 1998. Various Convention rights are likely to be relevant including:

- Article 8: Respect for your private and family life, home and correspondence Protocol 1,
- Article 1: Right to peaceful enjoyment of your property

11.2 This report has outlined the consultation that has been undertaken on the application and the opportunities for people to make representations to the Council as Local Planning Authority.

11.3 Members need to satisfy themselves that the potential adverse amenity impacts are acceptable and that any potential interference with the above Convention Rights will be legitimate and justified. Both public and private interests are to be taken into account in the exercise of the Local Planning Authority's powers and duties. Any interference with a Convention right must be necessary and proportionate. Members must therefore, carefully consider the balance to be struck between individual rights and the wider public interest.

12.0 CONCLUSION

12.1 Outline planning permission for the comprehensive mixed-use redevelopment of the application site was granted (subject to conditions and following completion of a Section 106 agreement) by the Mayor of London in March 2015. The outline planning permission set the parameters for the scale and massing of the development, the quantum and mix of floorspace to be provided and the overall layout of the site. This current application is for the approval of reserved matters in respect of the layout, scale, appearance, access and landscaping in respect of Plot 08, together with other details submitted for approval under conditions.

12.2 The Reserved Matters and application for approval under/discharge of conditions have been considered in the light of relevant policies and standards as well as representations from third parties. The Reserved Matters are considered to be in conformity with the approved development parameters for the scheme (scale, massing, floorspace, mix of uses, extent of public realm) and the submitted details, including those under conditions satisfactorily address the relevant policy considerations and other requirements, including the principles set out in Strategic Site Allocation in the Core Strategy.

12.3 Consideration has been given to the objections made to the proposed development, as set out in this report. It is considered that none of the material objections outweigh the reasons for approving the Reserved Matters and other details in respect of which approval is sought.

13.0 RECOMMENDATION

13.1 That the Committee resolve to:

- a) **GRANT** Reserved Matters approval (layout, scale, appearance, access and landscaping) in relation to development Plot 08 subject to completion of the legal agreement proposed at recommendation f) and the following conditions and informatives:
- b) **DISCHARGE** all other details and matters required to be approved under Condition 20(i) relation to Plot 08;
- c) **APPROVE DETAILS UNDER/DISCHARGE** conditions 3(ii), 7, 8, 13, 14(i), 15, 19, 20, 21(b),(c),(d),(e) and (f), 33, 45(i), 47 and 50(i)
- d) **PARTIALLY DISCHARGE** Condition 21(a), in relation to Plot 08 (to exclude approval of details of bus stops and associated passenger facilities which have yet to be submitted)
- e) **APPROVE** the external lighting strategy under Condition 12(i) in relation to the whole site.
- f) **AUTHORISE** the Director of Planning to negotiate and complete a deed of variation to the Section 106 Agreement dated 15 March 2015, under Section 106 of the 1990 Act (and other appropriate powers) so as to secure that the 65 London Affordable Rent units within Plot 15 are delivered concurrently with Plot 08.

13.2 That the Committee also authorise the Director of Planning to finalise and issue the decision notice in relation to the application and to include such amendments as she may consider appropriate to ensure the acceptable implementation of the development.

14.0 CONDITIONS

1. Approved Drawings and Documents

The development shall be carried out strictly in accordance with the application plans, drawings and documents hereby approved and as detailed below:

CON4-PA-03-011; CON4-PA-09-304; CON4-PA-09-303; CON4-PA-09-302; CON4-PA-09-301; CON4-PA-09-300; CON4-PA-07-101; CON4-PA-07-100; CON4-PA-03-013; CON4-PA-03-012; CON4-PA-05-303-A; CON4-PA-05-302-A; CON4-PA-05-301-A; CON4-PA-05-300-A; CON4-PA-05-204-A; CON4-PA-05-203-A; CON4-PA-05-202-A; CON4-PA-05-201-A; CON4-PA-05-109-A; CON4-PA-05-108-A; CON4-PA-05-107-A; CON4-PA-05-106-A; CON4-PA-05-105-A; CON4-PA-05-104-A; CON4-PA-05-103-A; CON4-PA-05-102-A; CON4-PA-05-101-F; CON4-PA-05-100-F; P10852-00-002-320 D00; P10852-00-002-130 D01; P10852-00-002-121 D01; P10852-00-002-120 D01; P10852-00-002-112 D01; P10852-00-002-111 D01; P10852-00-002-110 D01; P10852-00-002-109 D01; P10852-00-002-108 D01; P10852-00-002-105 D01; P10852-00-002-104 D01; P10852-00-002-103 D01; P10852-00-002-102 D01; P10852-00-002-101 D01; P10852-00-002-100 D01; CW-P08-GL-DS-0510-300-PL; P10466-00-002-800-D01; P10466-00-002-801-D01; P10852-00-002-100-MARK-UP-FOOTWAY-WIDTHS

Reason: To ensure that the development is carried out in accordance with the approved documents, plans and drawings submitted with the application and is acceptable to the local planning authority.

2. Wind Mitigation Measures

- a) Prior to occupation of Plot 08, a scheme of wind mitigation measures that are to be installed within terrace region 2 (being the roof terrace serving Flat 11.2.01, core 2

on Floor 11) at the site, shall be submitted to and approved in writing by the local planning authority.

- b) Any such mitigation as approved under part (a) shall be installed in accordance with the approved drawings, prior to first occupation and shall be retained and maintained permanently.

Reason: In order that the local planning authority may be satisfied that the development is safe with regard to wind and microclimate.

3. Design Response to Heritage Assets

Prior to the commencement of above ground development, full details of design response to heritage assets as outlined in, but not limited to, the document entitled "Convoys Wharf Plot 08, Supplementary Design Response to Heritage Assets" dated February 2020, shall be submitted to and approved by the local planning authority in consultation with Historic England (Greater London Archaeological Advisory Service). The approved details shall be implemented prior to occupation of the residential and commercial units and retained in perpetuity.

Reason: In order to demonstrate how the heritage assets of the site have informed design proposals.

15.0 INFORMATIVES

- A. **Positive and Proactive Statement:** The Council engages with all applicants in a positive and proactive way through specific pre-application enquiries and the detailed advice available on the Council's website. On this particular application, positive discussions took place which resulted in further information being submitted.
- B. You are advised that all construction work should be undertaken in accordance with the "London Borough of Lewisham Code of Practice for Control of Pollution and Noise from Demolition and Construction Sites" available on the Lewisham web page.
- C. The applicant is required to meet the relevant building control regulations in relation to the proposed development.